

APPENDIX E

Warehouse Point -

Planning for Stormwater Management
and Village Center Redevelopment Grant
(Warehouse Point Planning Study)

Public Participation December 2020 Part 1 of 4



LADA, P.C., Land Planners

WELCOME!



- This is the first in a series of four video presentations for the project known as
“The Planning for Stormwater and Village Center Redevelopment Grant for Warehouse Point”
OR
The Warehouse Point Planning Study
- The project is funded by a Responsible Growth and Transit Oriented Development Grant from the State of the Connecticut Department of Economic and Community Development (DECD)

This is a four part video series – each part runs approximately 20 minutes

- **Part One- Why do this study and what is it about?**
- Part Two- Understanding the Blue Ditch
- Part Three- Zoning and how it affect stormwater runoff
- Part Four – What the Future of Warehouse Point could look like

Comments on each segment can be posted on the Town's website at

<https://www.eastwindsor-ct.gov/planning-zoning-commission/webforms/warehouse-point-planning-study>

The comments will be collected and addressed at the Planning and Zoning Commission meeting of December 29, 2020.

There is a survey that is coordinated with this video series

- The survey is posted on the Town website at <https://www.eastwindsor-ct.gov/planning-zoning-commission/webforms/warehouse-point-planning-study>
- Surveys were also mailed to property owners within the study area
- Please only fill out the survey once per person but anyone in your household can do so including your children!

The final part of this public participation event is the Planning and Zoning Commission meeting on December 29, 2020. A Final Report with recommendations will be completed in January 2021

Welcome!

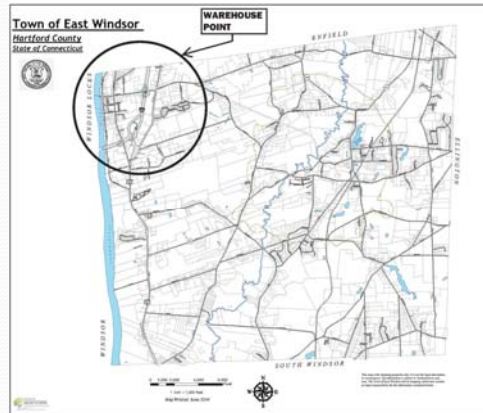


- Hi! My Name is Terri Hahn, Professional Landscape Architect
 - Licensed in CT and NY
 - Over 30 years of experience
 - Working in East Windsor for the last 5 + years

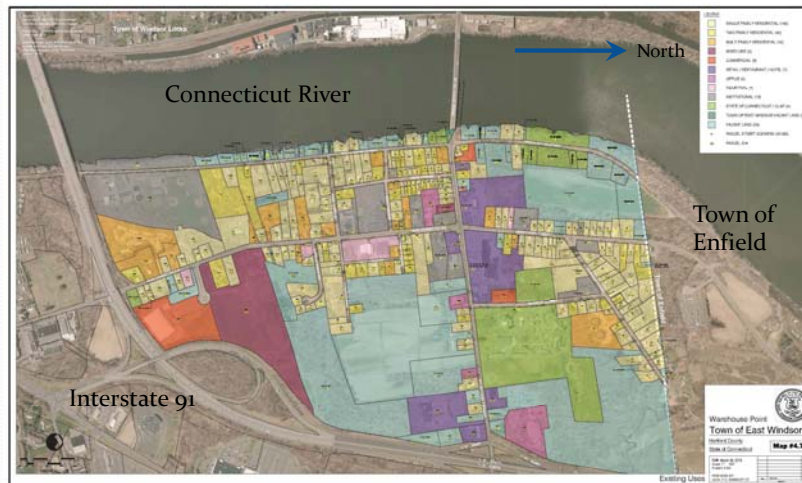
I am a Principal in the firm LADA, P.C. Land Planners from Simsbury, CT. We are the lead consultant for the project. We are Landscape Architects and Planners. The project engineer is Nathan L. Jacobson and Associates.

Introduction

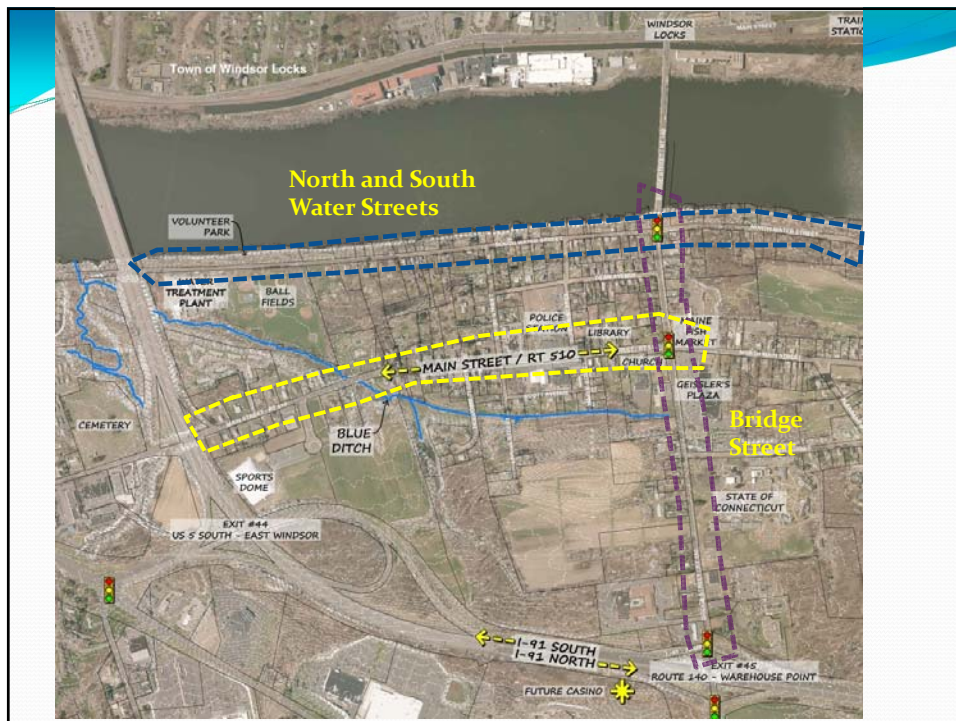
- Where is the project?



What parts of Warehouse Point are included in the study?



Lets look a little closer at the study area in Warehouse Point





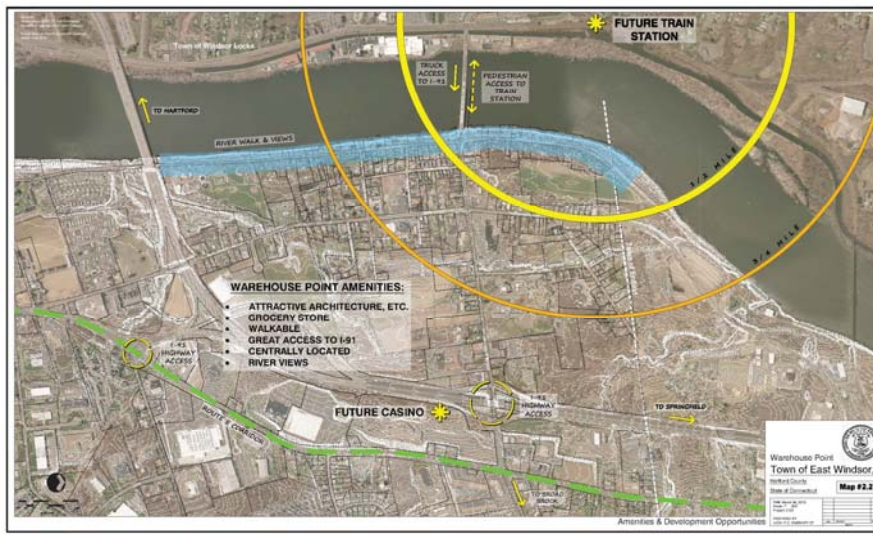
WHY IS THIS PROJECT IMPORTANT?



Town of East Windsor Planning

- Town of East Windsor Plan of Conservation and Development (PoCD), completed in 2016, identified the following:
 - In the resident survey of 2014 – 70% of respondents suggested the villages should be designated with new zoning to promote business and tourism
 - Page 20 and Page 29- Warehouse Point is identified as a separate Primary Strategy Area to define future Village Area development
 - Pages 40-41 and 54-57 identify over 30 recommendations to encourage business, retail and restaurant expansion in Warehouse Point
 - New Residential uses to provide “boots on the street” to support the new businesses in Warehouse Point are also recommended on the upper levels of existing historic structures and new construction
 - Page 20 identifies a strong desire to keep the historic character of Warehouse Point while encouraging new development

Why is this project important? Why did East Windsor get this grant?

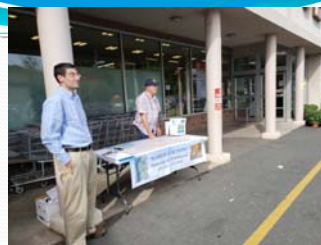


Warehouse Point Planning Study- Project Scope to date

- Applied for in 2017
- Granted in Summer of 2018
- Official Kick off meeting with State- June 2018
- Town authorized LADA to start Mid-September 2018
- Meetings with Town – October 2018- December 2020
- 12 month+ study of Blue Ditch- site observation and Drainage Calculations – stormwater model prepared which confirms observations and provides a means to quantify projections
- Existing Conditions Report Submitted – April 2019

Next

- Public Input –
 - Summer/Fall 2019
 - Most new homeowners have Flood Insurance as part of their mortgage
 - That may not be the case for property owners without mortgages
 - 2018- 2019 was a year where property owners were aware of flooding potential
 - Most respondents thought that –
 - Existing architecture and building edge limits should remain
 - Streetscape needed improvement
 - Need a reason to come to Warehouse Point- More stores/restaurants
 - New development as infill was preferable to tear down and start over

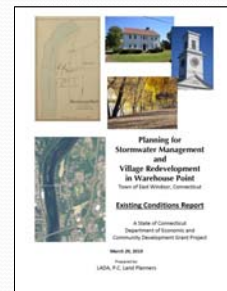


What do we mean by Existing Conditions?



Warehouse Point Planning Study- Existing Conditions Report-

- Base Maps Updated
- Data Gathering and Inventory Maps
 - Inventory Maps
 - Parcel Map including uses/with spreadsheet
 - Soils Map
 - Flood Limits Map
 - Wetland Maps
 - Existing Stormwater Facilities
 - Blue Ditch Documentation
 - River level records
 - Storm data
 - Existing Utilities, Parking, Pedestrian patterns
 - Zoning
 - Water and Sewage Usage



Existing Conditions

- Architecture Photo Record
- Coordination with past studies
- Current Zoning
- Existing Sidewalks
- Parcel layout and characteristics



There are 17 properties of 4+/- acres in size or larger with only 6 properties larger than 10 acres –

Town Sewer Plant and recreation Area- 17.27+/- acres

Field between North Main Street and North Water Street - 11.8+/- acres

Sports Complex at 202 Main Street - 26.47+/- acres

Connecticut State Boys Home- 25.45+/- acres

Parking Lot and vacant land at Hartsfield Corporate Center- 34.21+/- acres

Vacant Land between Holcomb Terrace and I-91 - 17.74+/- Acres

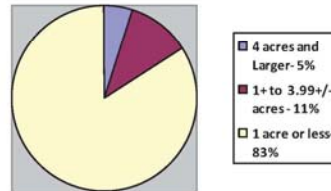


Figure 4-3 Properties by Acreage

There are 37 properties that are 1+/- acre- 3.99+/- acres in size with approximately 41% of them on Main Street.

There are 268 parcels of less than one acre in size. This represents 83% of the total parcels within Warehouse Point. Of these, 66% are 0.5+/- acres or less.

54 parcels at 0.5- 1+/- acres (16.7%)

112 parcels 0.25+/- acres to 0.5+/- acres (34.6%)

102 parcels 0-0.25+/- acres (31.7%)

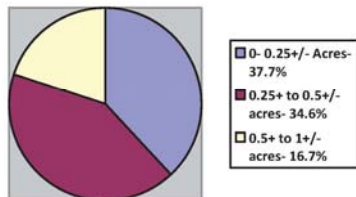


Figure 4-4 Properties less than 1+/- ac

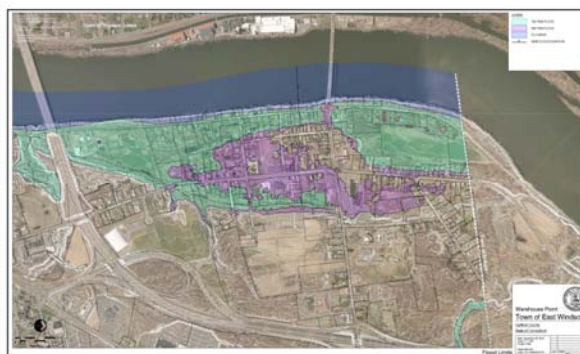
Parcels under 0.5 acres include most of Dean Avenue, Gardner Street, Holcomb Terrace, Scott Avenue, Spring Street, School Street and Pleasant Street.

Warehouse Point Planning Study- Existing Conditions

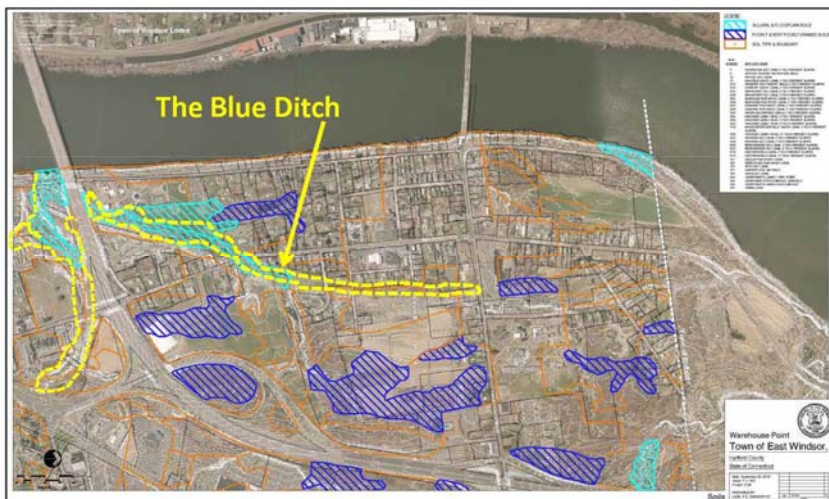
- Existing Stormwater Calculations
 - Understanding Stormwater vs. Flooding
 - Rain in East Windsor does not affect Connecticut River

- **Flooding in**

Warehouse Point has 2 causes- The Connecticut River (10 year storm+) And local Flooding due to the Blue Ditch



The Blue Ditch



Date: January 5, 2019, 3pm
Storm Event: 1.03" +/-
Timing: During Peak of Rain Event
Location: Bridge Street Crossing
Observations: Height and flow based on storm flow from watershed

Storm Event Photos
Warehouse Point TOD/Planning Project
The Blue Ditch and South Water Street

LADA, P.C. Land Planners 1/7/2019



Date: November 4, 2018, 3pm
Storm Event:
Timing: 8+/- hours after end of rain event
Location: Holcomb Terrace Crossing and Spring Street Crossing
Observations: Minimal flow from upstream conditions remaining after storm end.



Blue Ditch at Spring Street crossing - minimal flow

Storm Event Photos
Warehouse Point TOD/Planning Project
The Blue Ditch and South Water Street

LADA, P.C. Land Planners 12/28/2018



Date: December 13, 2018
Storm Event: none
Timing: low flow condition - river at 10-12'+/-
Location: South Water Street - Blue Ditch culverts
Observations: River Flow low- discharge pipe above river elevation

South Water Street elevation at 28' in this area.



Date: December 23, 2018, noon
Storm Event: 2+'' rain event
Observations: Pipe under water - above flood stage river at 20-22'

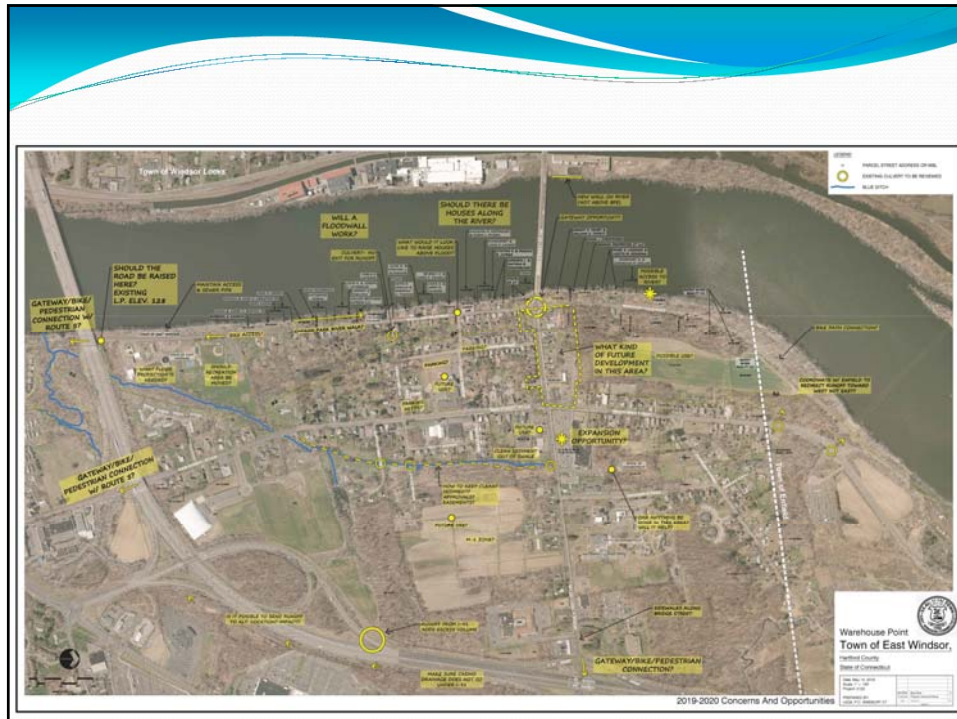
Storm Event Photos
Warehouse Point TOD/Planning Project
The Blue Ditch and South Water Street

LADA, P.C. Land Planners 1/7/2019

**The Existing Conditions Report
included a diverse group of initial recommendations
for further study as part of the project**

- Possible Recommendations
 - Consider a shared parking ordinance to provide flexibility and some protection to individual property owners
 - Consider a municipal parking lot for overflow of business and residential uses
 - Provide additional parking on South Water Street to connect to trails especially if recreation uses are expected to expand
 - Future use of the Dean Avenue neighborhood as a growth model should consider additional parking provisions
 - Extend Sidewalks to Route 5 via Main Street and Bridge Street
 - Establish Entry feature and gateway to Warehouse Point on Route 5 at Main Street and South Water Street, Establish Entry feature/Gateway on Bridge Street
 - Consider developing river access on North Water Street
 - Consider easements to town from properties where the Blue Ditch exists to allow for control of sediment and possibly storage expansion
 - Consider adding catch basins/yard drains to dewater rear of houses on South Water Street
 - Become proactive in coordinating with FEMA to take advantage of programs to purchase flood prone land

**From Here, the Planning
and Zoning Commission
compiled all their
thoughts into one map-
the Concerns and
Opportunities Map**



- What kind of Uses along Bridge Street?
- How to support the existing business ? Allow expansion?
- Future Sidewalk connections

- Impact of Flooding on the WWTP
- Can the road be raised?
- Is recreation in flood zone a good idea?



- Parking issues
- Additions to existing houses



**This is the end of Part One-
In the next video segment,
we look at where the Blue Ditch is,
how it collects stormwater, how it
is influenced by rain events and
what its relationship to the
Connecticut River is.
Please join us for Part 2.**

Thank you for your time!

- Comments can be made on the East Windsor Town Website-

<https://www.eastwindsor-ct.gov/planning-zoning-commission/webforms/warehouse-point-planning-study>

Comments will be collected from the Town website and addressed at the Planning and Zoning Commission meeting on December 29, 2020 at 6:30pm. Please check the website for Zoom log-in instructions. If you do not have Zoom, the Town posts all meetings after those meetings. You may contact the Planning Department if you have any questions.

We invite you to continue with Parts Two, Three and Four!!!

**Project funded by a grant from the
State of Connecticut
Department of Economic and Community
Development**


Copyright LADA, P.C. Land Planners November 2020
All rights reserved
Aerial Photos used in base maps from CTECO website
All photos - Terri Hahn
A LADA, P.C. Land Planners Production via Zoom platform recording

Terri Hahn
LADA, P.C. Land Planners
104 West Street
Simsbury, CT 06070
Email: ladapc@snet.net

Warehouse Point -

Planning for Stormwater Management
and Village Center Redevelopment Grant
(Warehouse Point Planning Study)

Public Participation December 2020 Part 2 of 4



LADA, P.C., Land Planners

WELCOME!



- This is the second in a series of four video presentations for the project known as
“The Planning for Stormwater and
Village Center Redevelopment Grant for
Warehouse Point”
OR
The Warehouse Point Planning Study
- The project is funded by a Responsible Growth and Transit Oriented Development Grant from the State of the Connecticut Department of Economic and Community Development (DECD)

This is a four part video series – each part runs approximately 20 minutes

- Part One- Why do this study and what is it about?
- **Part Two- Understanding the Blue Ditch – 30 minutes**
- Part Three- Zoning and how it affect stormwater runoff
- Part Four – What the Future of Warehouse Point could look like

Comments on each segment can be posted on the Town's website at

<https://www.eastwindsor-ct.gov/planning-zoning-commission/webforms/warehouse-point-planning-study>

The comments will be collected and addressed at the Planning and Zoning Commission meeting of December 29, 2020.

There is a survey that is coordinated with this video series

- The survey is posted on the Town website at <https://www.eastwindsor-ct.gov/planning-zoning-commission/webforms/warehouse-point-planning-study>
- Surveys were also mailed to property owners within the study area
- Please only fill out the survey once per person but anyone in your household can do so including your children!

The final part of this public participation event is the Planning and Zoning Commission meeting on December 29, 2020. A Final Report with recommendations will be completed in January 2021

Welcome!

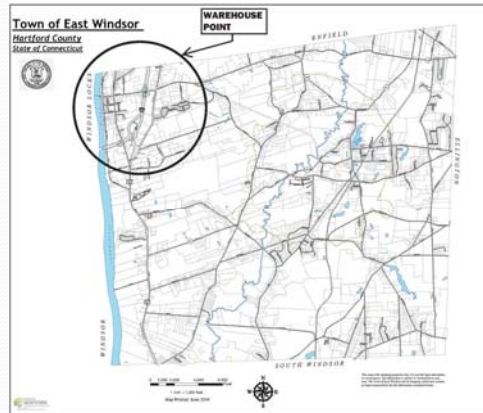


- Hi! My Name is Terri Hahn, Professional Landscape Architect
 - Licensed in CT and NY
 - Over 30 years of experience
 - Working in East Windsor for the last 5 + years

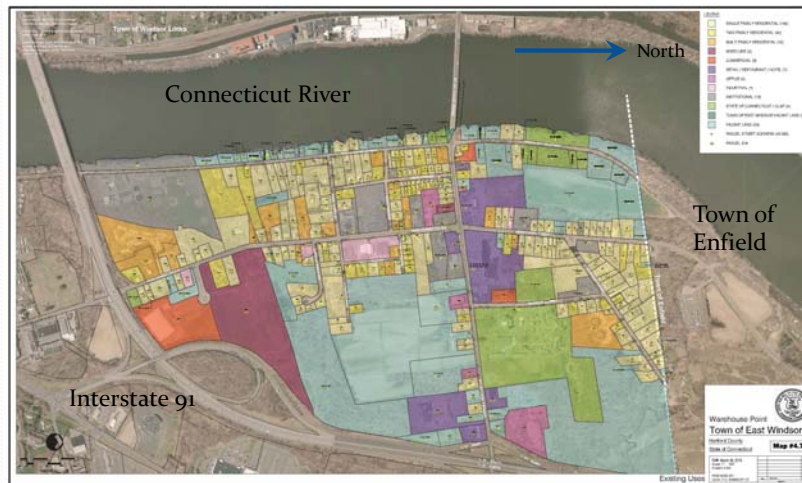
I am a Principal in the firm LADA, P.C. Land Planners from Simsbury, CT. We are the lead consultant for the project. We are Landscape Architects and Planners. The project engineer is Nathan L. Jacobson and Associates.

Introduction

- Where is the project?



What parts of Warehouse Point are included in the study?



WHY IS THIS PROJECT IMPORTANT?



Town of East Windsor Planning

- Town of East Windsor Plan of Conservation and Development (PoCD), completed in 2016, identified the following:
 - In the resident survey of 2014 – 70% of respondents suggested the villages should be designated with new zoning to promote business and tourism
 - Page 20 and Page 29- Warehouse Point is identified as a separate Primary Strategy Area to define future Village Area development
 - Pages 40-41 and 54-57 identify over 30 recommendations to encourage business, retail and restaurant expansion in Warehouse Point
 - New Residential uses to provide “boots on the street” to support the new businesses in Warehouse Point are also recommended on the upper levels of existing historic structures and new construction
 - Page 20 identifies a strong desire to keep the historic character of Warehouse Point while encouraging new development

Why is this project important? Why did East Windsor get this grant?



Warehouse Point Planning Study- Video Presentations so far

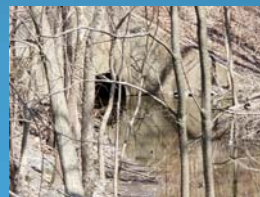
- Part 1 – reviewed study limits
- Reviewed Town of East Windsor Plan of Conservation and Development (PoCD) as it relates to Warehouse Point
- Established that residents would like to see a more lively and active Warehouse Point
- Reviewed work completed to date – Existing Conditions and initial public input

Part 2

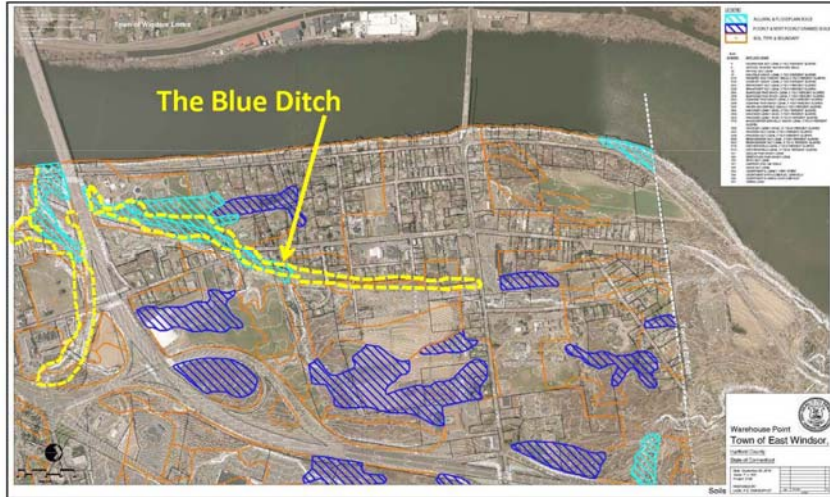
- What is the Blue Ditch?
- How does it relate to the Connecticut River?
- Where is the Blue Ditch and Why is the Blue Ditch important?
- What do we know about it?
- How does this affect future development?



What is the Blue Ditch?




The Blue Ditch




How does it relate to the Connecticut River?





Water level edge not visible

Date: December 13, 2018
 Storm Event: none
 Timing: low flow condition – river at 10-12’ +/-
 Location: South Water Street at Blue Ditch culverts
 Observations: River Flow low- discharge pipe above river elevation




Date: December 23, 2018, noon
 Storm Event: 2+” rain event
 Observations: Pipe under water – above flood stage river at 20-22’


South Water Street elevation at 28’ in this area.

Storm Event Photos
Warehouse Point TOD/Planning Project
The Blue Ditch and South Water Street

LADA, P.C. Land Planners 1/7/2019



Date: December 13, 2018
 Storm Event: none



Date: December 23, 2018, noon
 Storm Event: 2+” rain event

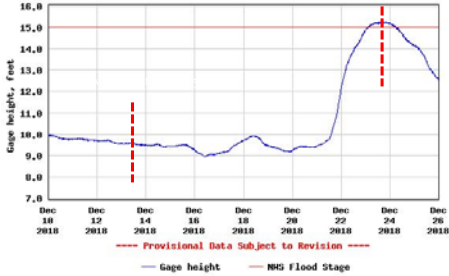
Flood Level as defined by National Weather Service - 15’

Compare elevation on gauge with map-

December 13 – topo map 10-12’ gauge – 9.5’ above river bottom at 3’ +/-

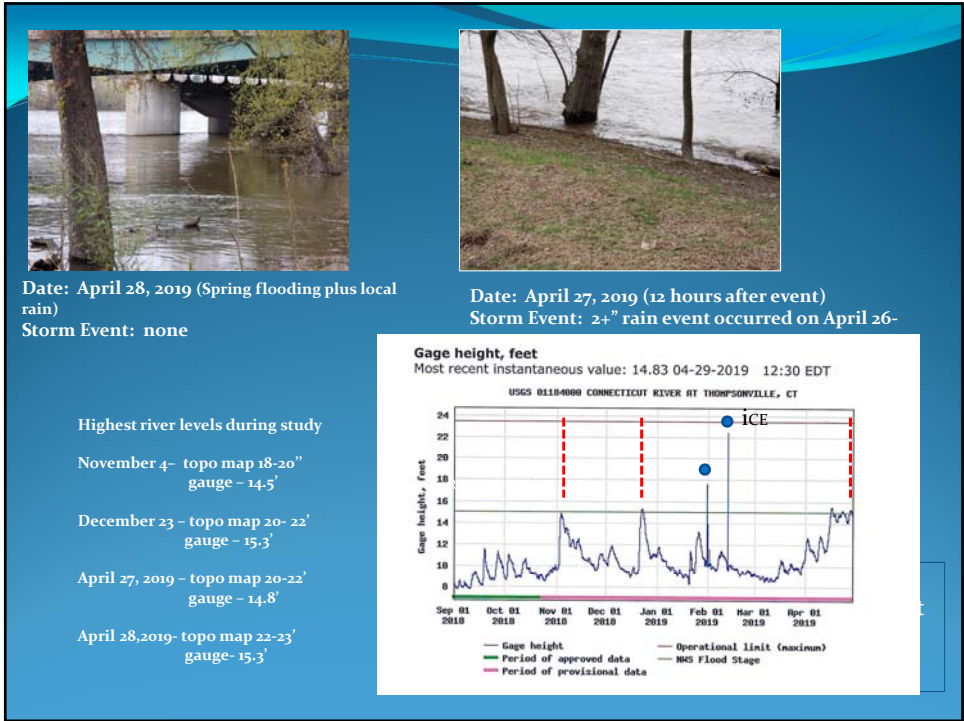
December 23 – topo map 20- 22’ gauge – 15.3’ above river bottom at 3’ +/-

Gage height, feet
 Most recent instantaneous value: 9.93 01-08-2019 08:30 EST
 USGS 01184000 CONNECTICUT RIVER AT THOMPSONVILLE, CT



----- Provisional Data Subject to Revision -----

— Gage height — NWS Flood Stage



Conclusions

Connecticut River watershed much larger than local

Any storm above a 10 year storm flooding is due to Connecticut River not the Blue Ditch

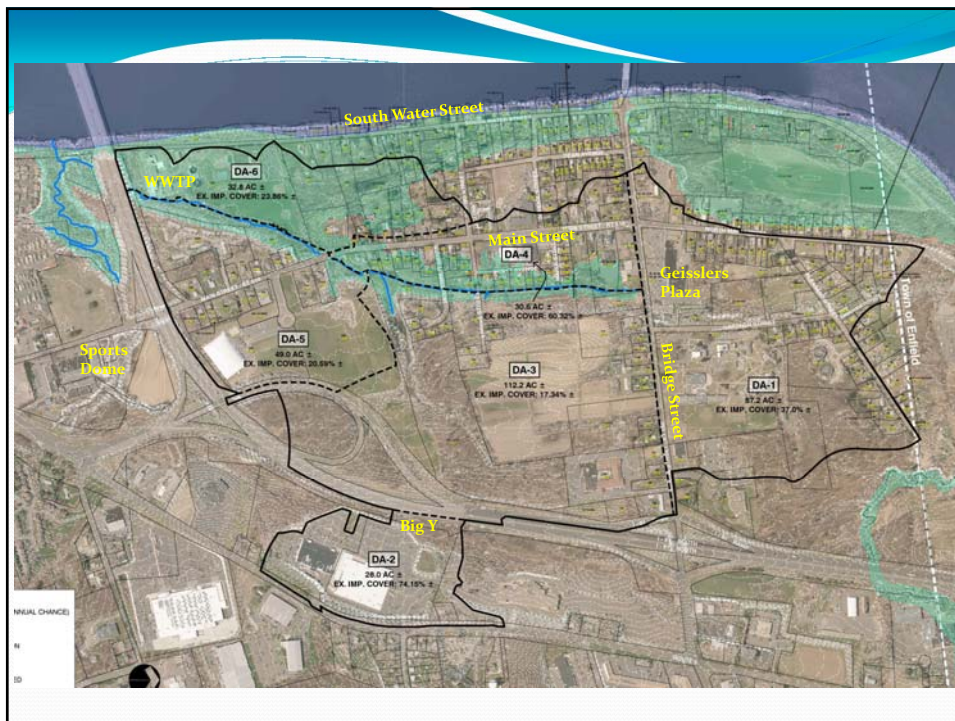
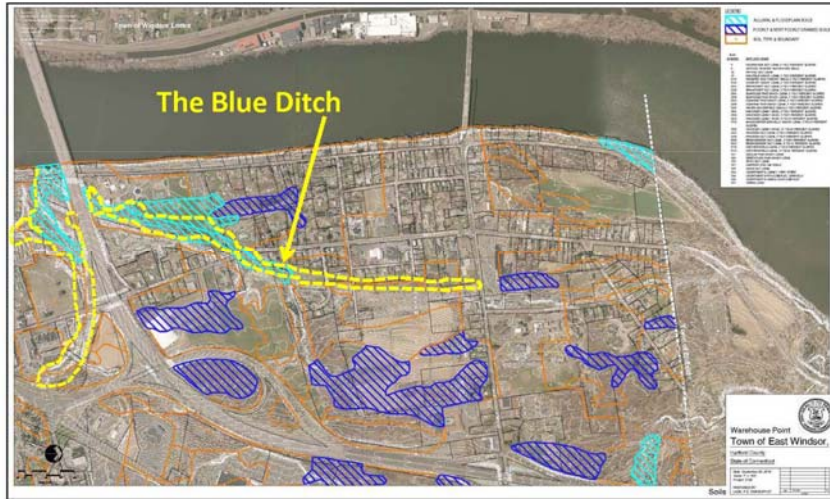
Blue Ditch drains to Connecticut River but has limited impact on River

Connecticut River affects Blue Ditch when it reaches elevation above 24’ as it causes a backwater to occur and no flow leaves the Blue Ditch.

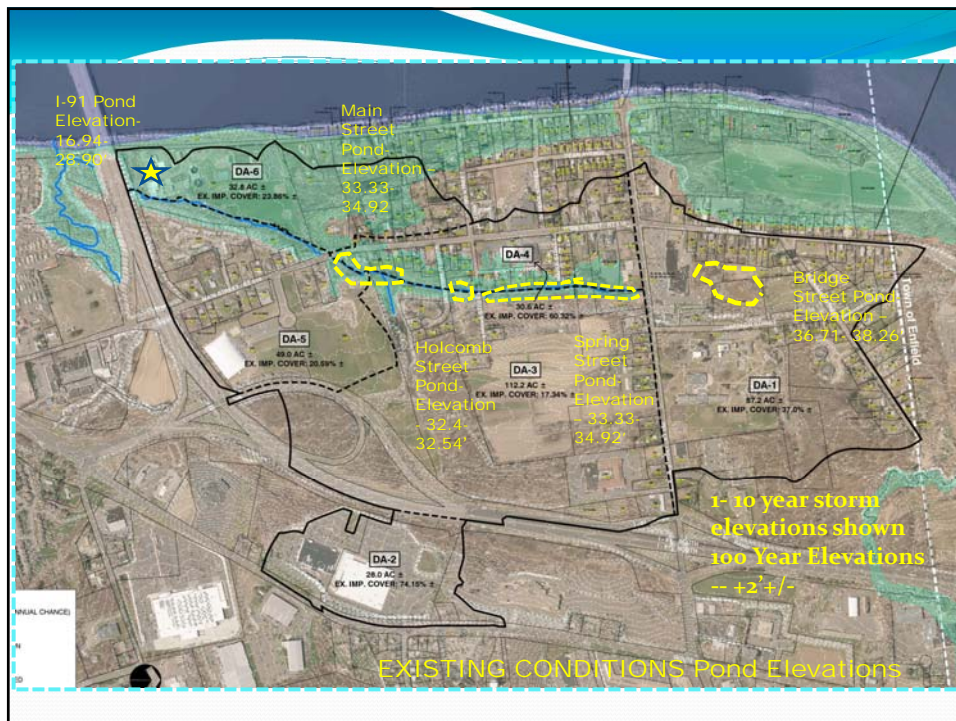
River Gauge at Thompsonville is not quite synched with topo but whenever gauge reaches 15’ – expect flooding along river.


Flood Level at elevation 22-23’
River Gauge at 15.3’
April 28, 2019

What parts of Warehouse Point drain to The Blue Ditch?




Let's look at the parts of the Blue Ditch






**Bridge Street Pond
North of Geisslers
Parking lot at rear of
building. Low Flow
level at Elevation
35.0
Parking Lot at Elev.
39'**

High Water Elevation at Elev. 38'+/-



Flow from Bridge Street, Geisslers and land to north




Date: February 6, 2019
Storm Event: Low flow
-No rain in several days.
Culvert under Bridge Street under water.
Note sediment build up to above height of pipe

Date: January 5, 2019, 3pm
Storm Event: 1.03'+/-
Timing: During Peak of Rain Event
Location: Bridge Street Crossing
Observations: Height and flow based on storm flow from watershed


Storm Event Photos
Warehouse Point TOD/Planning Project
The Blue Ditch and South Water Street

LADA, P.C. Land Planners 1/7/2019 Rev. 2/10/2019


**Spring Street Pond
Northside-
Bridge Street to Spring
Street- Outlet/Low Flow
level at Elev. 31.5**




High Water level shown at Elev. 36'+ on Church Property




**Spring Street Pond
Southside
Outlet/Low Flow at
Elev. 31.5'**



High Water Level at Elev. 34'+/-




Low Flow Level




Holcomb Street Pond Spring to Holcomb Outlet/Low Flow Level – Elevation 31.5'

Water Elevation at Elev. 33'+/-




High Water Level at Elev. 36'+/-



Low Flow Level

Sheet 1 of 7



Blue Ditch at
Holcomb Terrace
looking south-
minimal flow



Blue Ditch at Spring
Street crossing-
minimal flow

Date: November 4, 2018, 3pm
Storm Event:
Timing: 8+/- hours after end of rain event
Location: Holcomb Terrace Crossing and Spring
Street Crossing
Observations: Minimal flow from upstream
conditions remaining after storm end.

Storm Event Photos
Warehouse Point TOD/Planning Project
The Blue Ditch and South Water Street

LADA, P.C. Land Planners 12/28/2018



Low Flow Level

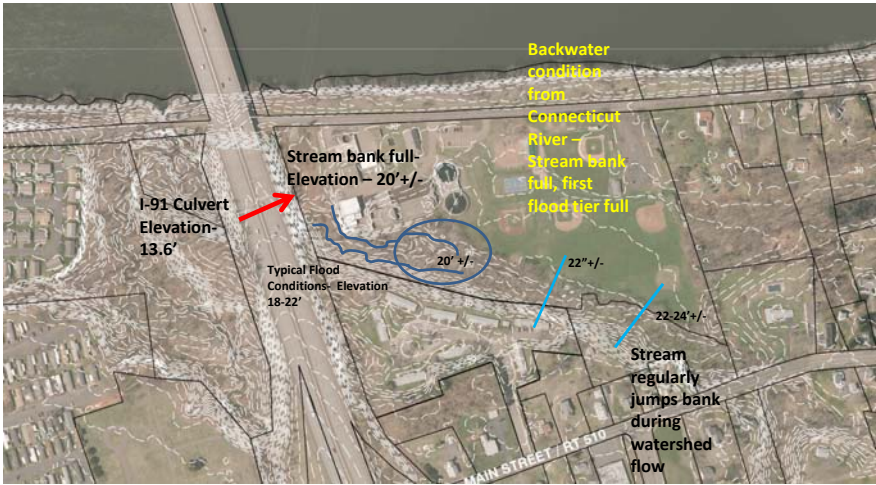
Main Street Pond- - Outlet Elevation 27' Elevation at Holcomb Street -30'



Water Elevation
at 32'



Main Street
culvert
crossing



I-91 Culvert
Elevation-
13.6'

Stream bank full-
Elevation - 20'+/-

Typical Flood
Conditions: Elevation
18-22'

20' +/-

22' +/-

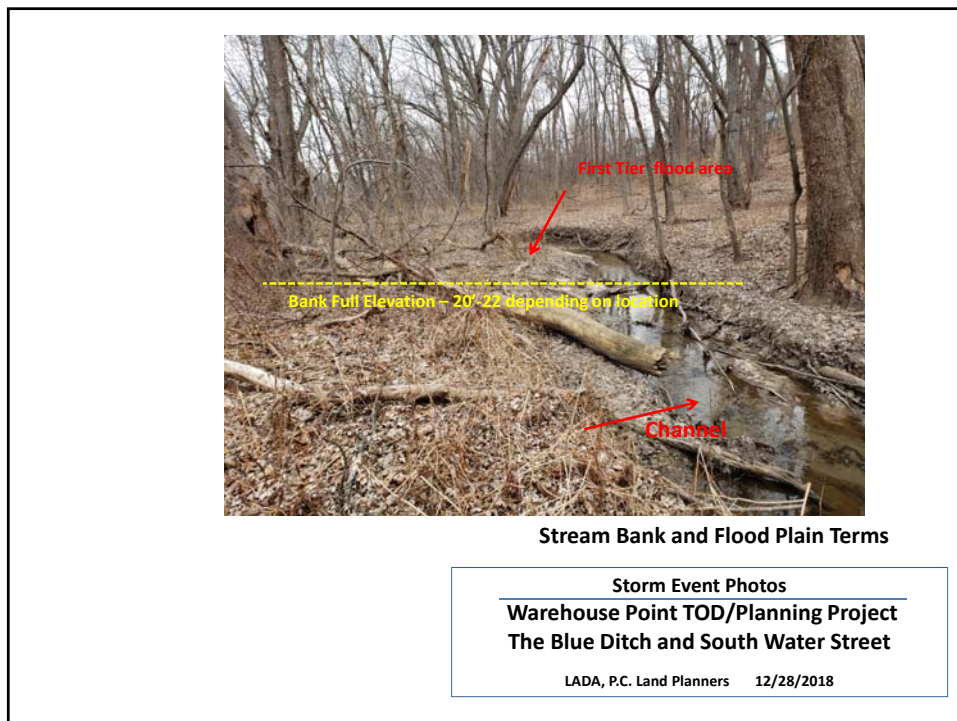
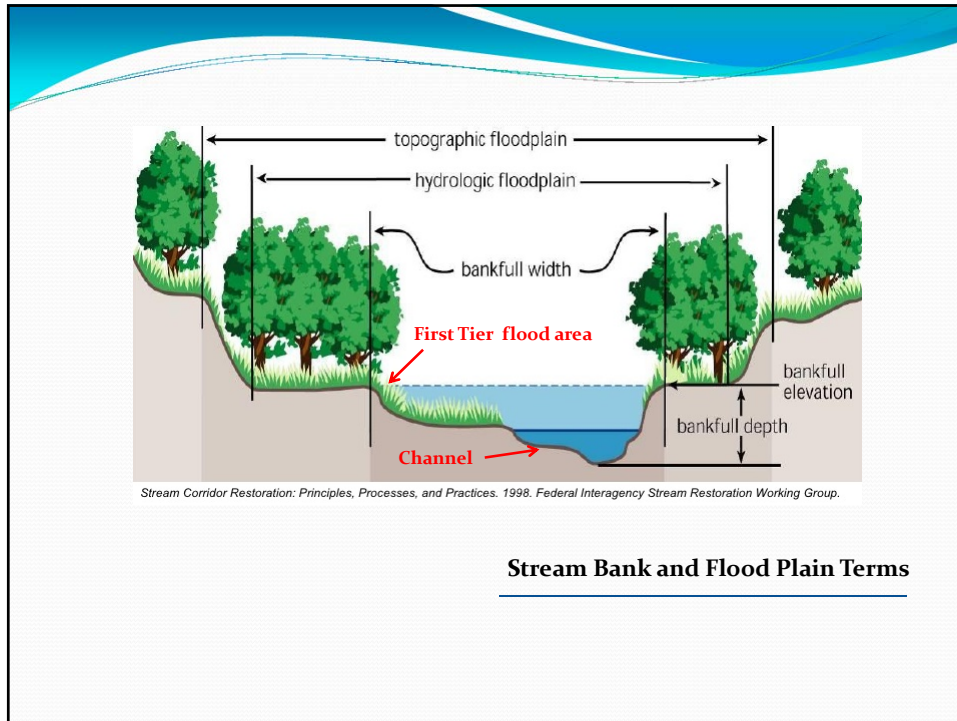
22-24' +/-

Stream
regularly
jumps bank
during
watershed
flow

Backwater
conditions
from
Connecticut
River -
Stream bank
full, first
flood tier full

MAIN STREET AT 910

I -91 Pond Outlet Elevation – 13.6'





Date: November 4, 2018, 3pm
 Storm Event:
 Timing: 8+/- hours after end of rain event
 Location: The Blue Ditch at the Little League Fields
 Observations: Peak of water level from upstream watershed is approximately 6-12 hours prior to observation. Peak of water level for Connecticut River occurred just after visit. Approximate water level of watershed flow/flood limit = elevation 21/22 feet+-.

Storm Event Photos
Warehouse Point TOD/Planning Project
The Blue Ditch and South Water Street

LADA, P.C. Land Planners 12/28/2018



Date: November 4, 2018, 3pm
 Storm Event:
 Timing: 8+/- hours after end of rain event
 Location: The Blue Ditch at the Little League Fields between fields and WWTP
 Observations: Backwater condition at elevation 20 feet +/- due to height of river- no flow observed.

Storm Event Photos
Warehouse Point TOD/Planning Project
The Blue Ditch and South Water Street

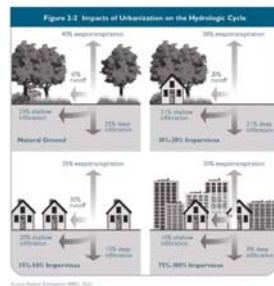
LADA, P.C. Land Planners 12/28/2018

Now that we understand where the Blue Ditch is and how it reacts during a storm, the next step is to understand what will happen in future storms.

So what does this all have to do with Future Development in Warehouse Point?

IMPERVIOUS SURFACE TIED DIRECTLY TO STORMWATER RUNOFF

Connecticut Stormwater Quality Manual (2004)



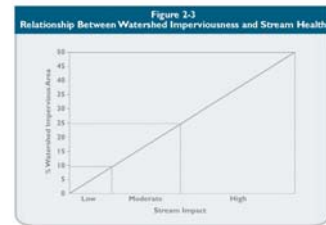
**Table 2-2
Typical Impervious Coverage
of Land Uses in the Northeast U.S.**

Land Use	% Impervious Cover
Commercial and Business Districts	85-95
Industrial	75-85
High Density Residential	45-60
Medium Density Residential	25-45
Low Density Residential	20-40
Open Areas	0-10

Source: MAHRT, 1997; Kaufman and Bost, 1990; Arnold and Giblin, 1996; and Conservation Service, 1975.

The impacts of development on stream ecology can be grouped into four categories:

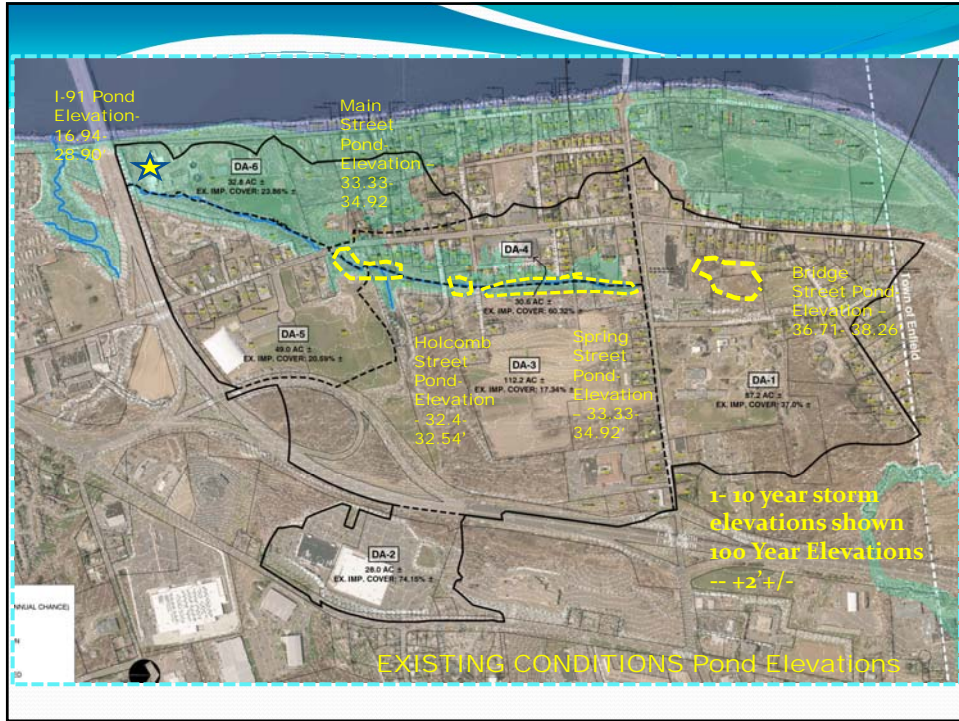
1. Hydrologic Impacts
2. Stream Channel and Floodplain Impacts
3. Water Quality Impacts
4. Habitat and Biological Impacts




Source: Adapted from Schmalzer, 1992 and Storm Water's Effects, Woodward, 1995.

Current Conditions - Stormwater Runoff Calculations and Model

- Existing Impervious Surface
 - All storms above the 10 year storm impacted by Connecticut River. No change/improvement likely despite calculations
 - 5 Drainage Areas and I-91 Culvert
 - Existing Impervious Cover shown on Map
 - Drainage Area 1- North of Bridge Street (27%)
(Bridge Street Pond)
 - Drainage Area 3- Eastern Portion of Bridge Street (17%)
(Eastern portions of Spring Street Pond, Holcomb Street Pond and Main Street Pond)
 - Drainage Area 4- Main Street North (60%)
(Western Portions of Spring Street Pond, Holcomb Street Pond and Main Street Pond)
 - Drainage Area 5- Main Street South (21%)
(I-91 Pond)
 - Drainage Area 6- South Water Street (23%)
(I-91 Pond)



The stormwater model confirms the field observations



The slide features a large blue background with a white wavy border at the top. The main text is in a large, bold, yellow font. In the bottom right corner, there is a small, square photograph showing a stream with brown water flowing through a wooded area with snow on the ground.

Can we do anything to improve the existing conditions?

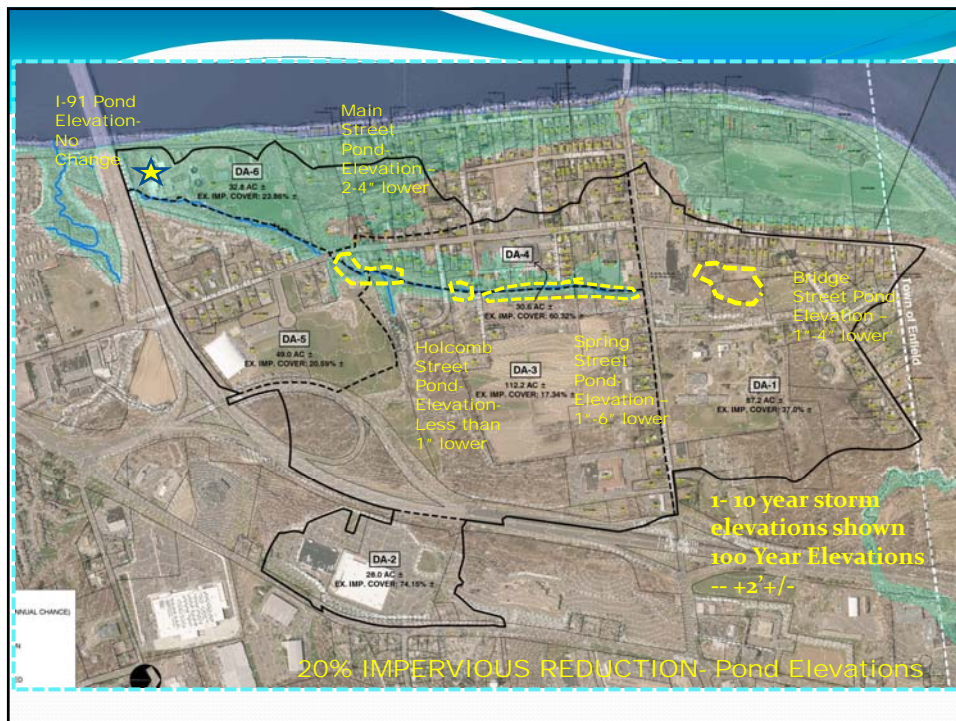
ALTERNATE #1- Replace the existing culverts

ALTERNATE #2- Reduce Impervious Surface

Culvert Replacement will Improve the pond elevations at the Spring Street and Holcomb Street Ponds but will increase the elevations at the 1-91 Pond because the water will get there sooner.

Post-Development Stormwater Runoff Calculations and Model

- 20% Reduction of Existing Impervious Surface
 - Some reductions in peak flow but, surprisingly, not really enough to justify cost
 - Actual elevation of water level reductions- 1-2” below existing levels typical- more at Spring Street.

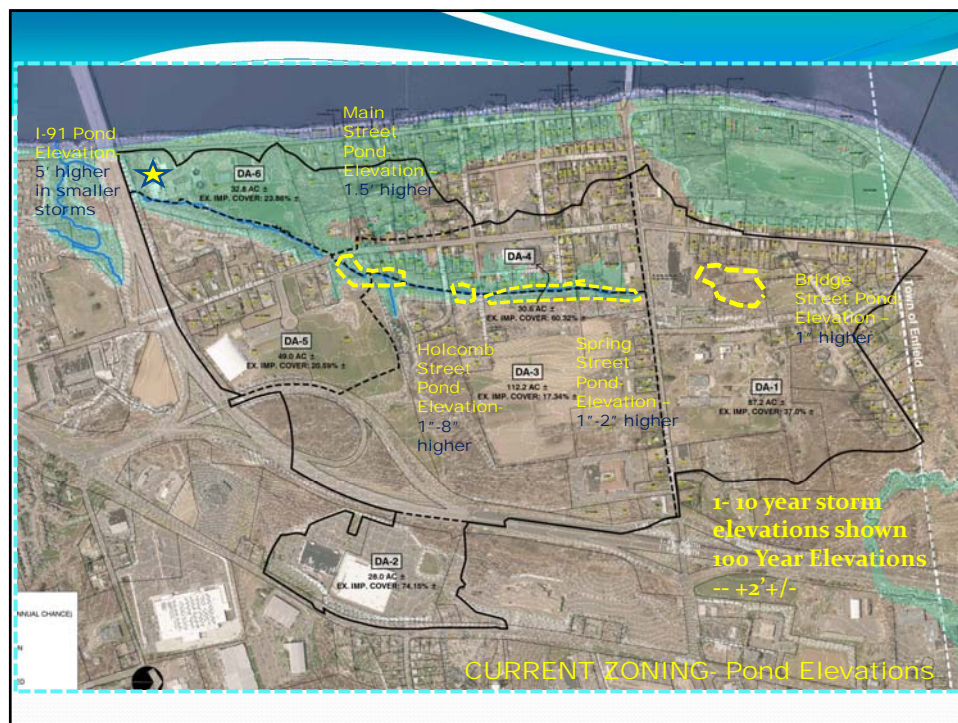


**If you had to remove 20%
of the impervious surface
on your property-
driveway, parking and or
building- Is it worth it?**

So what happens next?

Post-Development Stormwater Runoff Calculations and Model

- Existing Zoning
 - M₁ Zone - 75% Impervious Cover Allowed
 - B₁/B₂ Zone- 65% Impervious Cover Allowed
 - R₁ Zone- 25% Impervious Cover Allowed
 - Total Impervious Cover impacted by wetlands, setbacks, access location, etc.
 - Location of M and B zones at points where have impact on stormwater
 - Drainage Areas 3 , 4 , 5 most impacted



Post-Development Stormwater Runoff Calculations and Model

- Alternates being considered
 - Impact of significant Maintenance- culvert cleaning
 - Replace Pipes
 - Modify Proposed Zoning
 - Requiring infiltration /pervious pavement for new construction
 - Reduce impervious in certain locations
 - Detention???

Questions to consider:

1. What do you think should be done to improve the runoff situation in the Blue Ditch?
2. Should new development be allowed in Warehouse Point even if it increases runoff?
3. Should the Town invest in stormwater infrastructure improvements to allow future development to occur ?
4. Should property owners in the area pay a special tax to cover those improvements?
5. Should new projects pay an extra fee to offset their contribution to the stormwater improvements?
 6. How likely are these new fees to cause new development to go elsewhere?

**This is the end of Part Two-
In the next video segment,
we look how zoning is affected by
impervious surface and how future
development can be
accommodated without causing
substantial increases in runoff.
Please join us for Part 3.**

Thank you for your time!

- Comments can be made on the East Windsor Town Website-

<https://www.eastwindsor-ct.gov/planning-zoning-commission/webforms/warehouse-point-planning-study>

Comments will be collected from the Town website and addressed at the Planning and Zoning Commission meeting on December 29, 2020 at 6:30pm. Please check the website for Zoom log-in instructions. If you do not have Zoom, the Town posts all meetings after those meetings. You may contact the Planning Department if you have any questions.

We invite you to continue with Parts Three and Four!!!

**Project funded by a grant from the
State of Connecticut
Department of Economic and Community
Development**


Copyright LADA, P.C. Land Planners November 2020
All rights reserved
Aerial Photos used in base maps from CTECO website
All photos - Terri Hahn
A LADA, P.C. Land Planners Production via Zoom platform recording

Terri Hahn
LADA, P.C. Land Planners
104 West Street
Simsbury, CT 06070
Email: ladapc@snet.net

Warehouse Point -

Planning for Stormwater Management
and Village Center Redevelopment Grant
(Warehouse Point Planning Study)

Public Participation December 2020 Part 3 of 4



LADA, P.C., Land Planners

WELCOME!



- This is the third in a series of four video presentations for the project known as
“The Planning for Stormwater and Village Center Redevelopment Grant for Warehouse Point”
OR
The Warehouse Point Planning Study
- The project is funded by a Responsible Growth and Transit Oriented Development Grant from the State of the Connecticut Department of Economic and Community Development (DECD)

This is a four part video series – each part runs approximately 20 minutes

- Part One- Why do this study and what is it about?
- Part Two- Understanding the Blue Ditch
- **Part Three- Zoning and how it affects stormwater runoff**
- Part Four – What the Future of Warehouse Point could look like

Comments on each segment can be posted on the Town's website at <https://www.eastwindsor-ct.gov/planning-zoning-commission/webforms/warehouse-point-planning-study>

The comments will be collected and addressed at the Planning and Zoning Commission meeting of December 29, 2020.

There is a survey that is coordinated with this video series

- The survey is posted on the Town website at <https://www.eastwindsor-ct.gov/planning-zoning-commission/webforms/warehouse-point-planning-study>
- Surveys were also mailed to property owners within the study area
- Please only fill out the survey once per person but anyone in your household can do so including your children!

The final part of this public participation event is the Planning and Zoning Commission meeting on December 29, 2020. A Final Report with recommendations will be completed in January 2021

Welcome!

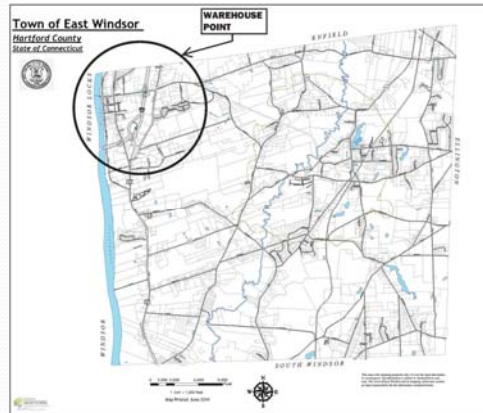


- Hi! My Name is Terri Hahn, Professional Landscape Architect
 - Licensed in CT and NY
 - Over 30 years of experience
 - Working in East Windsor for the last 5 + years

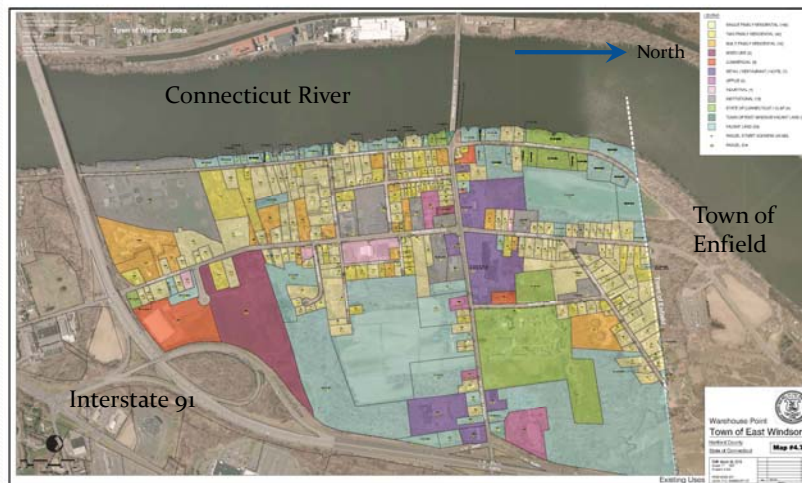
I am a Principal in the firm LADA, P.C. Land Planners from Simsbury, CT. We are the lead consultant for the project. We are Landscape Architects and Planners. The project engineer is Nathan L. Jacobson and Associates.

Introduction

- Where is the project?



What parts of Warehouse Point are included in the study?



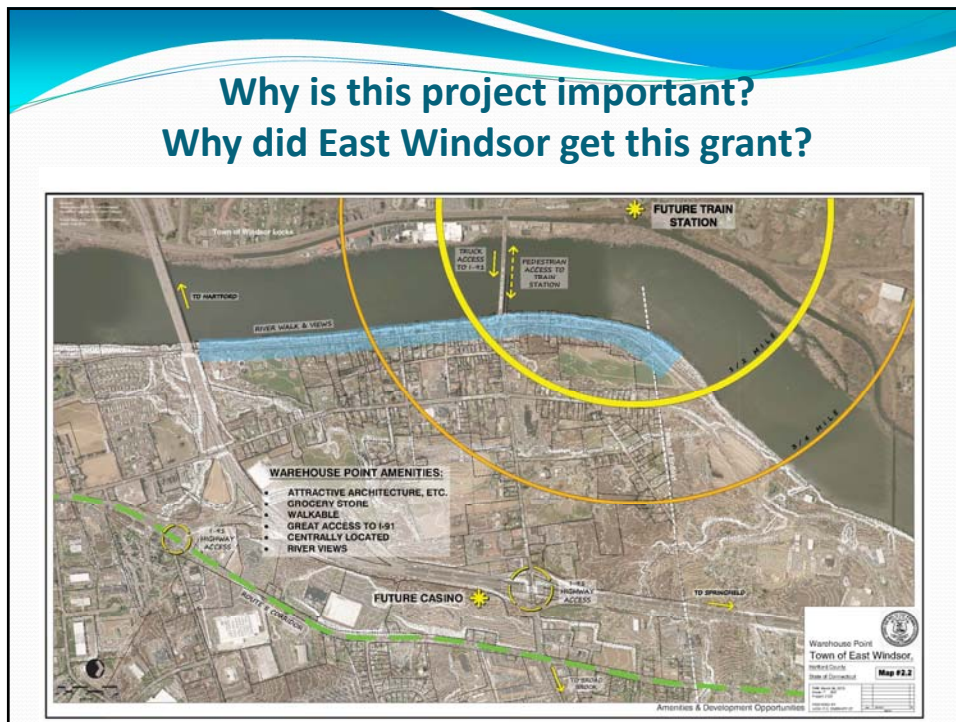
WHY IS THIS PROJECT IMPORTANT?



Town of East Windsor Planning

- Town of East Windsor Plan of Conservation and Development (PoCD), completed in 2016, identified the following:
 - In the resident survey of 2014 – 70% of respondents suggested the villages should be designated with new zoning to promote business and tourism
 - Page 20 and Page 29- Warehouse Point is identified as a separate Primary Strategy Area to define future Village Area development
 - Pages 40-41 and 54-57 identify over 30 recommendations to encourage business, retail and restaurant expansion in Warehouse Point
 - New Residential uses to provide “boots on the street” to support the new businesses in Warehouse Point are also recommended on the upper levels of existing historic structures and new construction
 - Page 20 identifies a strong desire to keep the historic character of Warehouse Point while encouraging new development

Why is this project important? Why did East Windsor get this grant?



Warehouse Point Planning Study- Video Presentations so far

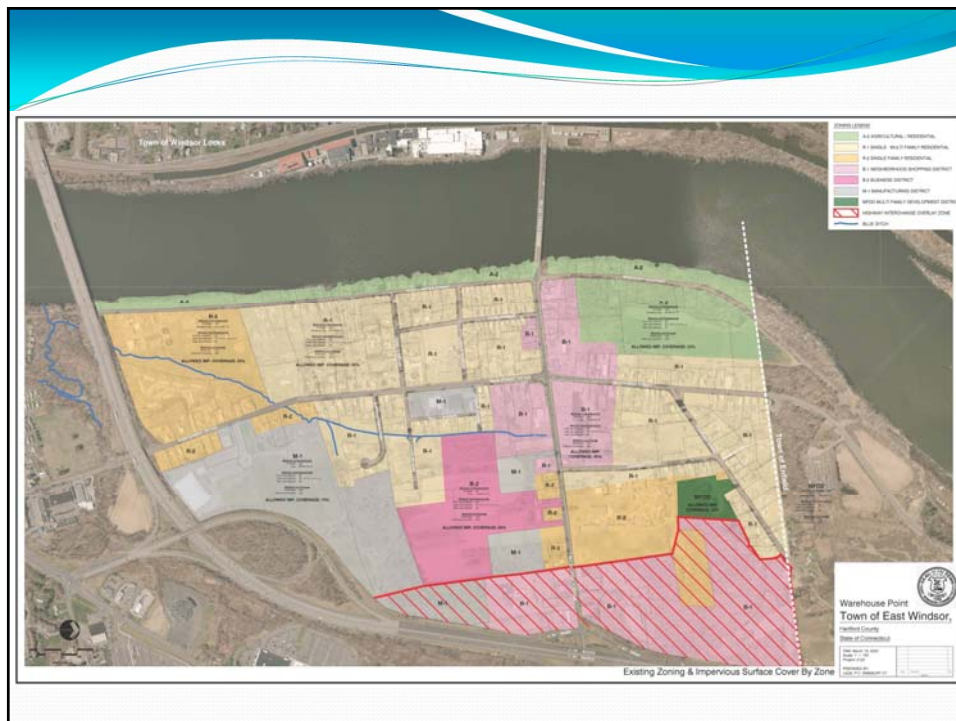
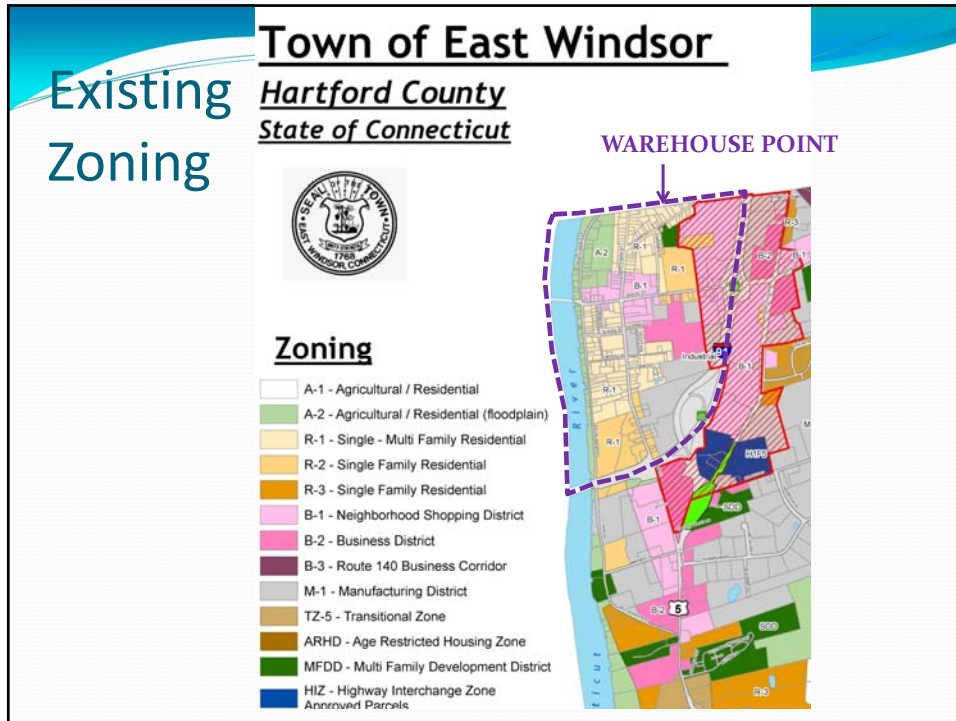
- Part 1 – reviewed study limits
- Part 2- looked at stormwater runoff in Warehouse Point
- How stormwater runoff is connected to the Connecticut River but how Connecticut River flooding is not directly connected to stormwater runoff from Warehouse Point
- Reviewed what and where the Blue Ditch works and how it is impacted by existing impervious surface.

Part 3

- What is the current zoning for Warehouse Point?
- What obstacles to new development opportunities are there in in the current Zoning for Warehouse Point?
- How does zoning impact stormwater runoff ?
- What kinds of things can we do to allow new development and minimize future runoff?

What is the current Zoning?





Identified Issues with Existing Zoning

- Existing Residential Lots Non-conforming
- No Growth potential for existing businesses
- Need to look at Warehouse Point as a whole and not just parcels
- Uses not reflective of community needs and wants
- Not flexible
- No architectural guidelines
- Growth needs to reflect historic patterns and architecture
- This is not Route 5

What do we want Warehouse Point to become?

- Vibrant
- Exciting
- Some Place you want to go
- Some Place you want to live
- Walkable
- Connected
- A Place with History and a Future!



What parts of the Zoning Regulations reflect what the community wants for future development?

**Bulk Requirements- Setbacks, % Impervious Surface, etc
Use
Architectural Guidelines
Submission requirements and process.**

Bulk Requirements

- Building Setbacks
- Parking Setbacks
- Building Coverage - % of lot allowed to be covered by building
- Impervious Coverage - % of lot allowed to be covered by impervious surface
 - (Impervious surface means any material covering the ground that does not allow water to percolate into the soil. This includes buildings, asphalt (tar, macadam, Etc), concrete, stone or pavers and compacted gravel)

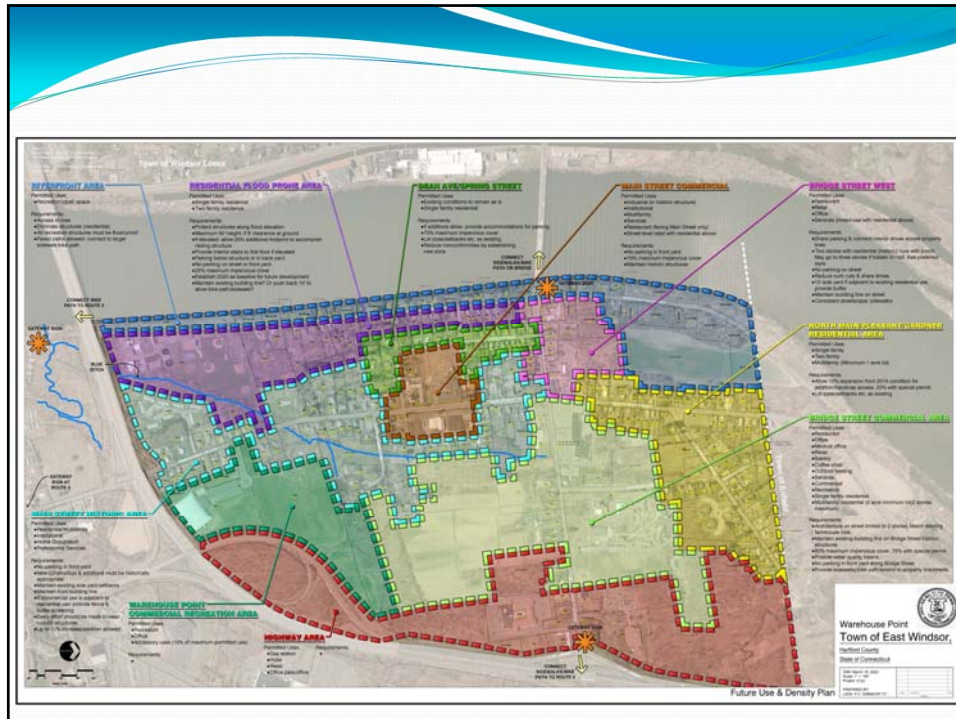


Architectural Guidelines

- In Connecticut, the Town can not mandate specific architecture.
- Guidelines to show the kinds of things/styles the people prefer are allowed
- Exceptions
 - Village District – allows for detailed architectural review but is considered another layer of regulation
 - Form-Based Code – very detailed and specific based on series of plans and images



The Wish List-
Based on input from residents for the PoCD and throughout this project, The Planning and Zoning Commission put together a plan entitled “Future Use and Density Plan” to depict what they believe the residents would like to see for the future



The Future Use and Density Plan identifies areas for both future growth and where limited growth, if any, should occur.

RIVERFRONT AREA

Permitted Uses:

- Recreation/open space

Requirements:

- Access to river
- Eliminate structures (residential)
- All recreation structures must be flood-proof
- Paved paths allowed- connect to larger sidewalk/bike path

Town of Windsor Locks

RIVERFRONT AREA

Permitted Uses:

- Recreation/open space

Requirements:

- Access to river
- Eliminate structures (residential)
- All recreation structures must be flood-proof
- Paved paths allowed- connect to larger sidewalk/bike path

RESIDENTIAL FLOOD PRONE AREA

Permitted Uses:

- Single family residence
- Two family residence

Requirements:

- Protect structures along flood elevation
- Maximum 50' height- if 8' clearance at ground
- If elevated- allow 25% additional footprint to accomplish raising structure
- Provide interior stairs to first floor if elevated
- Parking below structure or in back yard
- No parking on street or front yard
- 25% maximum impervious cover
- Establish 2020 as baseline for future development
- Maintain existing building line? Or push back 10' to allow bike path/sidewalk?

DEAN AVE/SPRING STREET

Permitted Uses:

- Existing conditions to remain as is
- Single family residential

Requirements:

- All structures allow- provide accommodations for parking
- 25% maximum impervious cover
- All communications etc. as existing
- Structure nonconformities by establishing new zone

MAIN STREET COMMERCIAL

Permitted Uses:

- Industrial (on historic structure)
- Retail
- Hospitality
- Services
- Warehouse (along Main Street only)
- Street level retail with residential above

Requirements:

- No parking on front yard
- 25% maximum impervious cover
- Maintain historic structures

RESIDENTIAL FLOOD PRONE AREA

Permitted Uses:

- Single family residence
- Two family residence

Requirements:

- Protect structures along flood elevation
- Maximum 50' height- if 8' clearance at ground
- If elevated- allow 25% additional footprint to accomplish raising structure
- Provide interior stairs to first floor if elevated
- Parking below structure or in back yard
- No parking on street or front yard
- 25% maximum impervious cover
- Establish 2020 as baseline for future development
- Maintain existing building line? Or push back 10' to allow bike path/sidewalk?

Town of Windsor Locks

RIVERFRONT AREA

Permitted Uses:

- Recreation/open space

Requirements:

- Access to river
- Eliminate structures (residential)
- All recreation structures must be flood-proof
- Paved paths allowed- connect to larger sidewalk/bike path

RESIDENTIAL FLOOD PRONE AREA

Permitted Uses:

- Single family residence
- Two family residence

Requirements:

- Protect structures along flood elevation
- Maximum 50' height- if 8' clearance at ground
- If elevated- allow 25% additional footprint to accomplish raising structure
- Provide interior stairs to first floor if elevated
- Parking below structure or in back yard
- No parking on street or front yard
- 25% maximum impervious cover
- Establish 2020 as baseline for future development
- Maintain existing building line? Or push back 10' to allow bike path/sidewalk?

DEAN AVE/SPRING STREET

Permitted Uses:

- Existing conditions to remain as is
- Single family residential

Requirements:

- All structures allow- provide accommodations for parking
- 25% maximum impervious cover
- All communications etc. as existing
- Structure nonconformities by establishing new zone

MAIN STREET COMMERCIAL

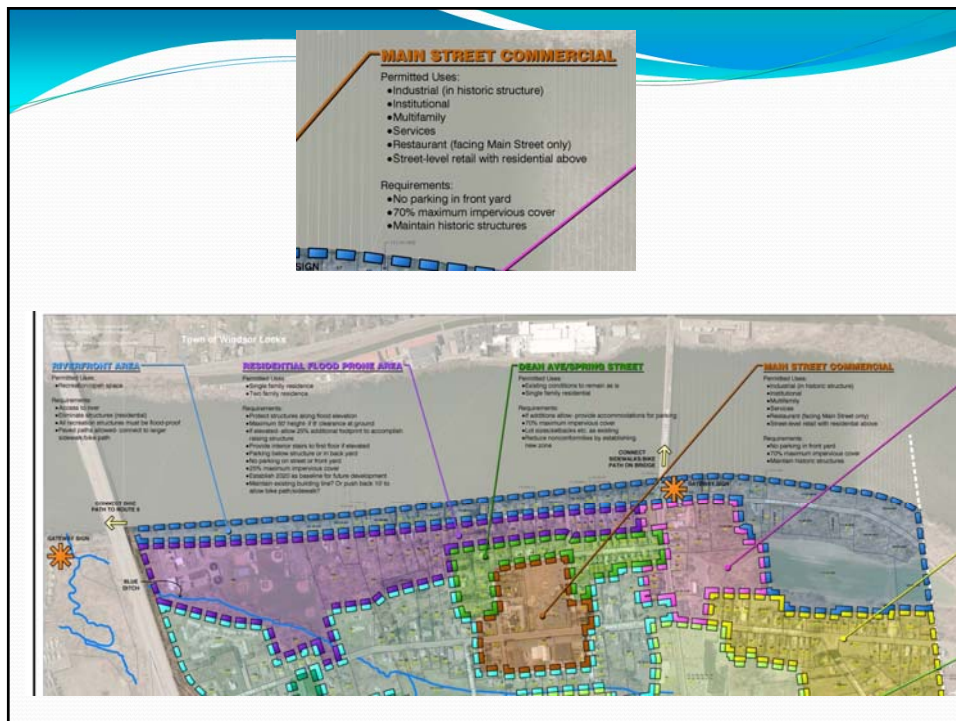
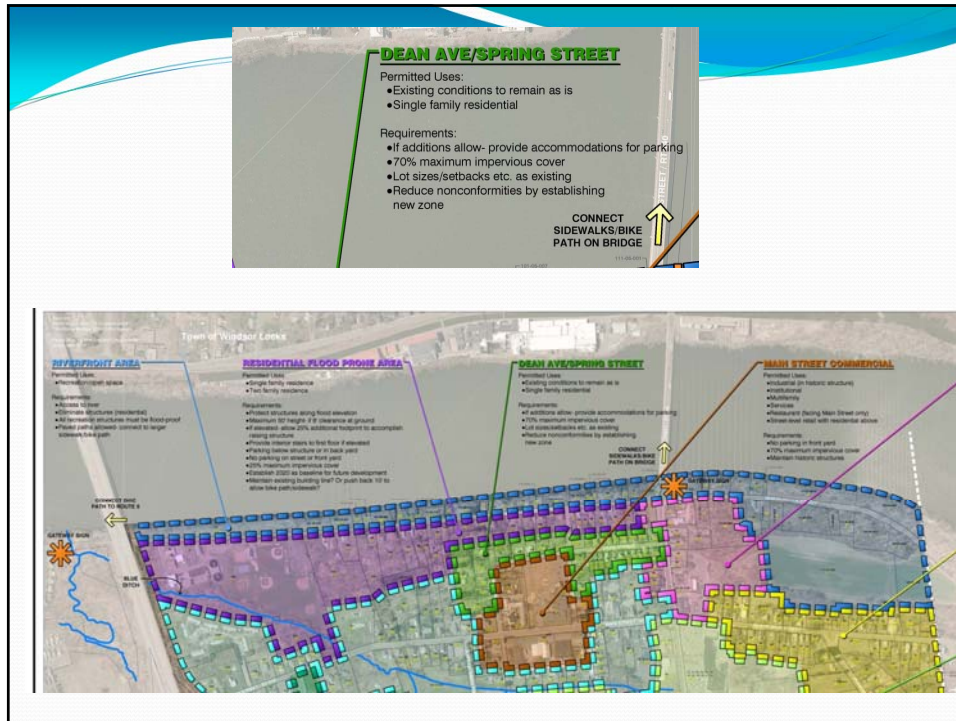
Permitted Uses:

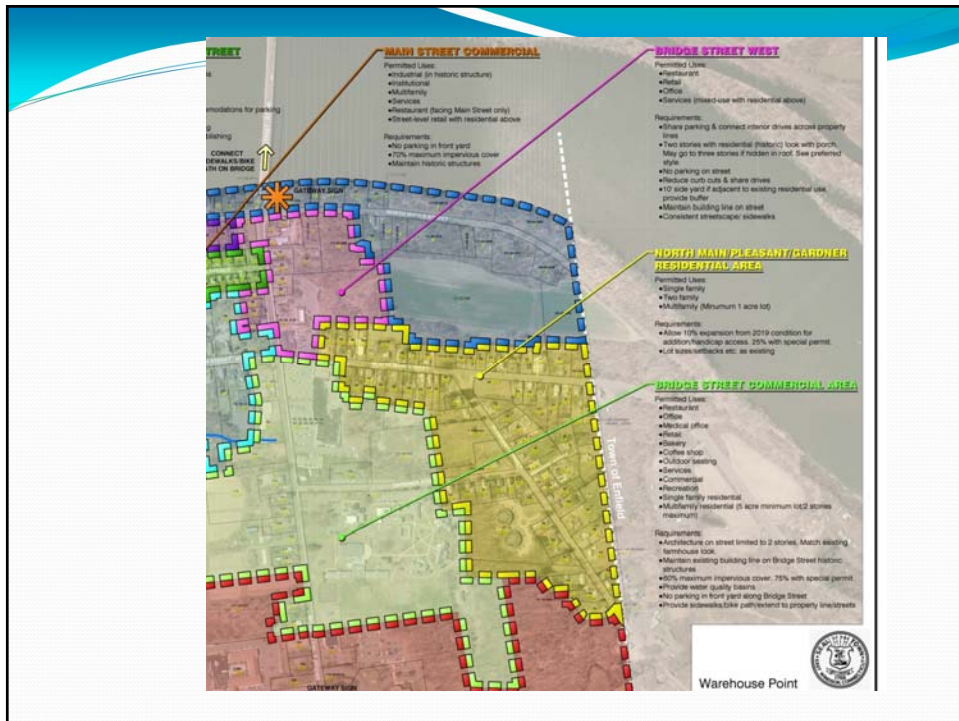
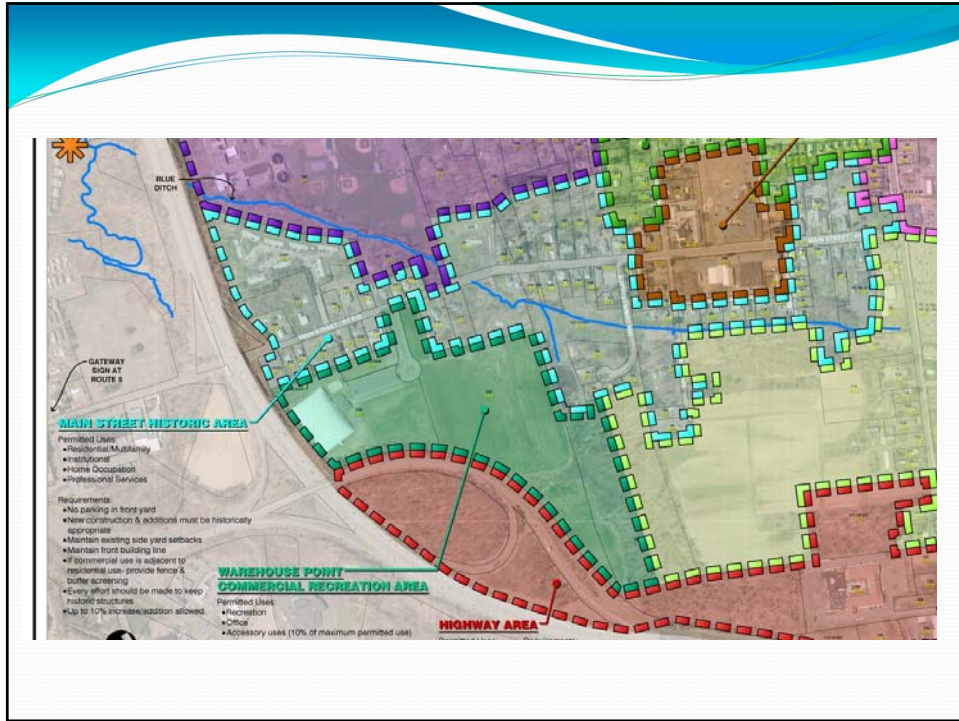
- Industrial (on historic structure)
- Retail
- Hospitality
- Services
- Warehouse (along Main Street only)
- Street level retail with residential above

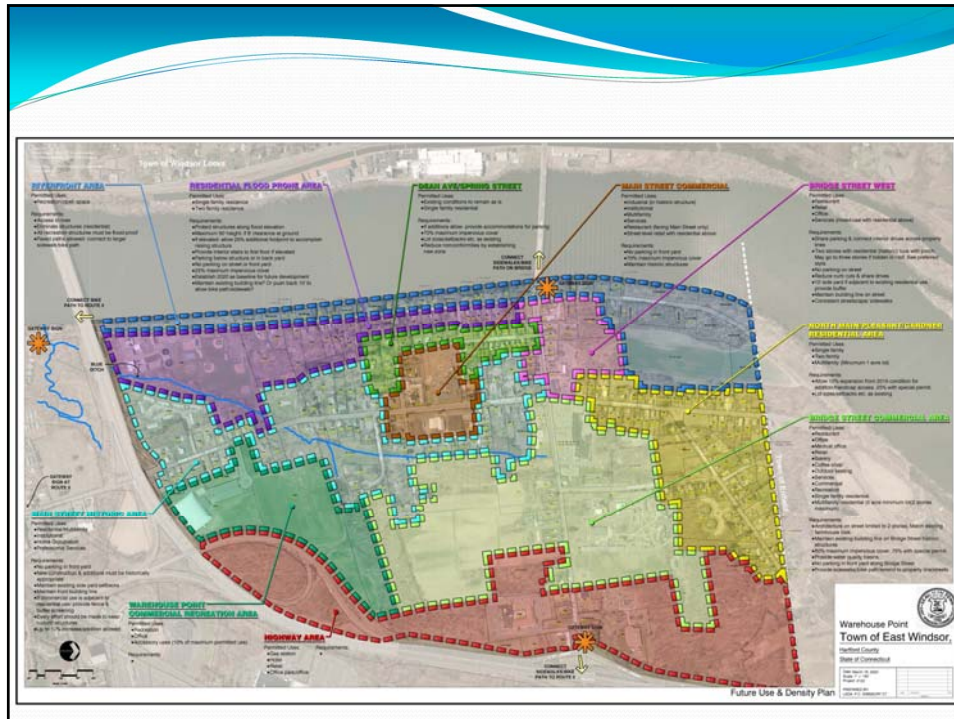
Requirements:

- No parking on front yard
- 25% maximum impervious cover
- Maintain historic structures

13







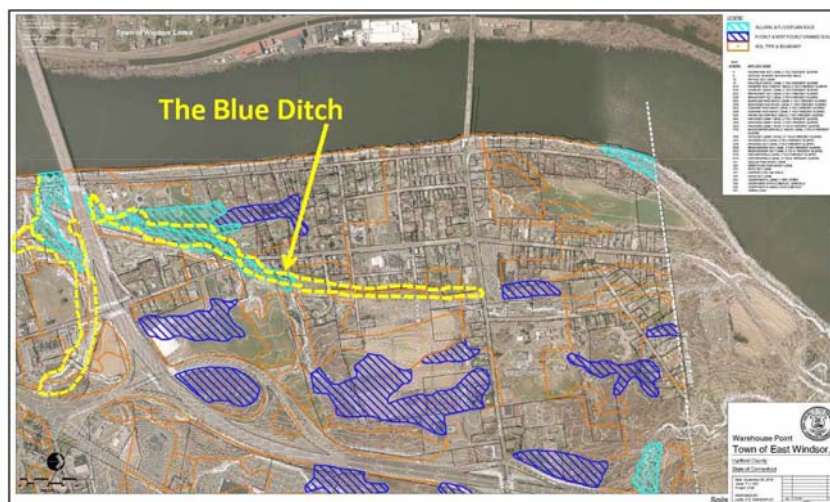
Zoning and its impact on Stormwater

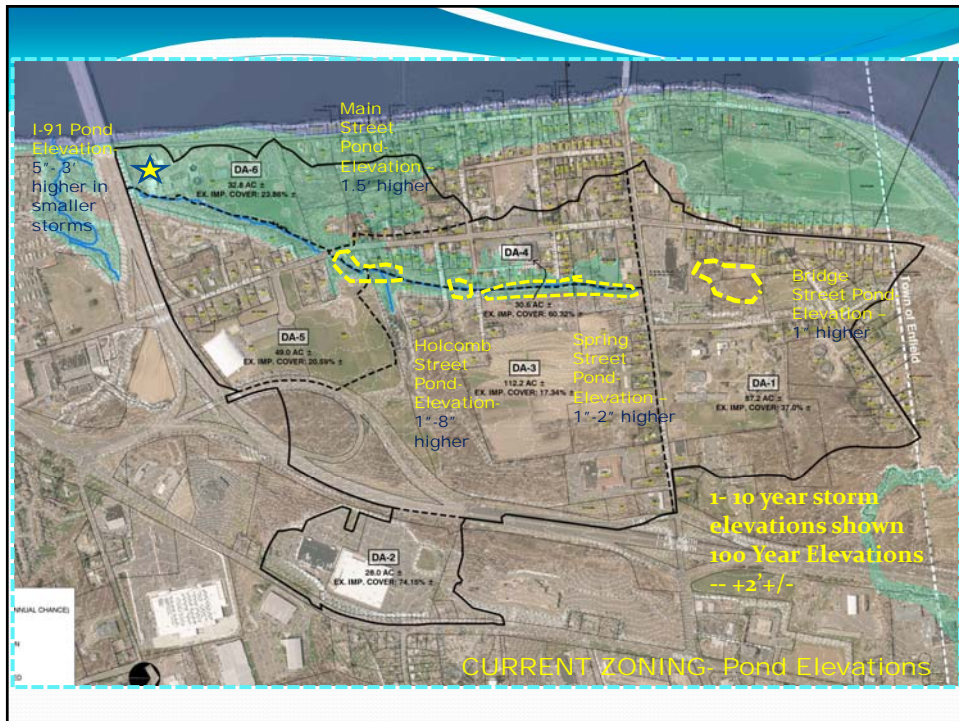
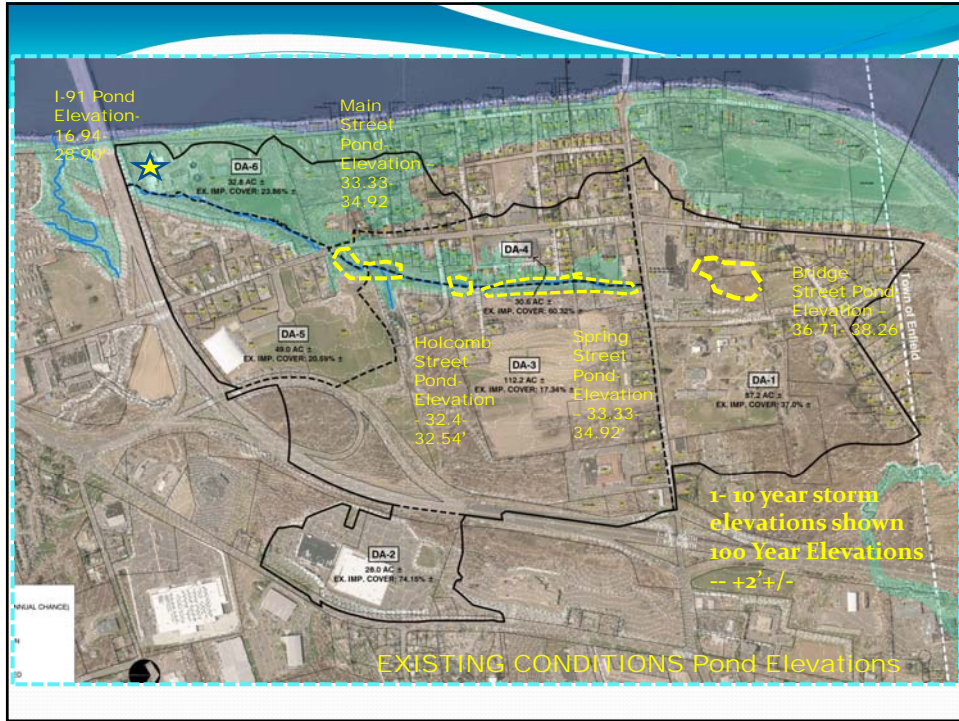
- Stormwater Regulations
- Flood Zone Regulations
- Building Code
- Zoning Regulations-
 - Use dictates site plan layout, parking requirements, etc
 - Bulk Requirements- setbacks, “build-to lines”, FAR allowed, impervious surface, open space

Why does it matter?

- Every time you build a building or add on or create a new parking area, you increase the rate and volume of stormwater runoff from the site.
- Either the Blue Ditch or the Connecticut River will receive this runoff.
- Increased impervious surface may have an impact on the Blue Ditch by increasing the amount of water collected each storm at any one time at each of the various ponds identified in Part 2 of this series.
- You are not allowed to increase runoff across your property line

The Blue Ditch



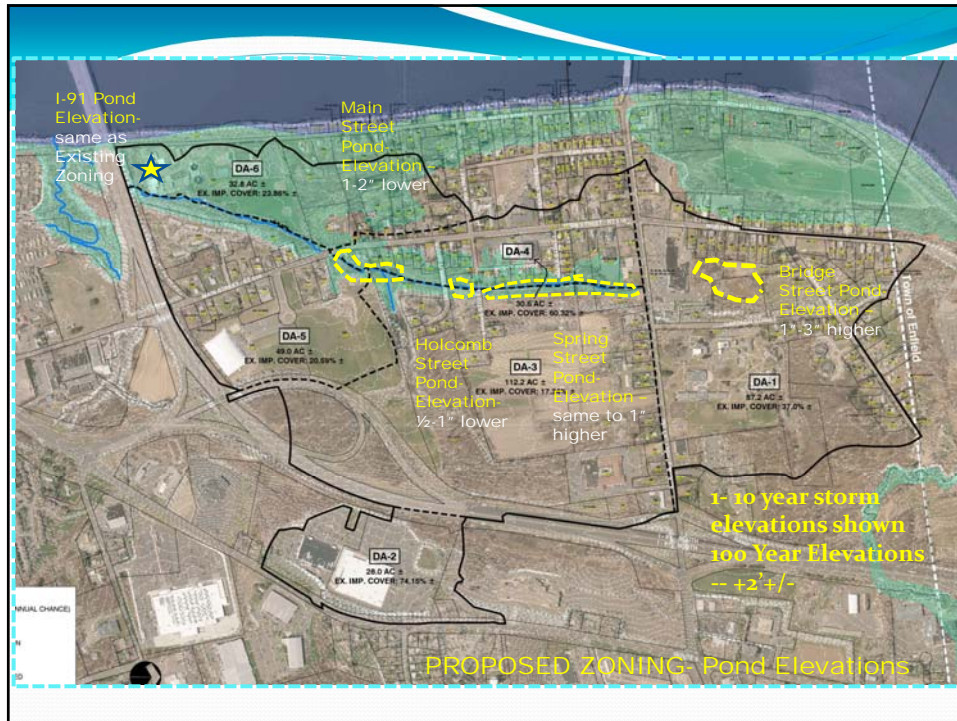


Planning and Zoning Commission Goals for the project

- Minimize potential increases to stormwater runoff but allow for suitable uses and growth
- Consider creative ideas that can be implemented on both a large and small scale
- Look at long term and short term solutions

Post-Development Stormwater Runoff Calculations and Model

- Alternates being considered
 - Impact of significant Maintenance- culvert cleaning
 - Replace Pipes
 - Consider Community improvements that might affect whole area
 - Requiring infiltration /pervious pavement for new construction
 - Reduce impervious in certain locations, including long term removal of structures
 - Detention in specific locations



Questions to consider:

1. Do you think properties in Warehouse Point should be allowed to have more growth?
2. Should new development be allowed in Warehouse Point even if it increases runoff?
3. Do the goals set for this project by the Planning and Zoning Commission make sense to you?

+

**This is the end of Part Three-
In the next video segment,
we consider what the future of
Warehouse Point might look like.
Please join us for Part 4.**

Thank you for your time!

- Comments can be made on the East Windsor Town Website-

<https://www.eastwindsor-ct.gov/planning-zoning-commission/webforms/warehouse-point-planning-study>

Comments will be collected from the Town website and addressed at the Planning and Zoning Commission meeting on December 29, 2020 at 6:30pm. Please check the website for Zoom log-in instructions. If you do not have Zoom, the Town posts all meetings after those meetings. You may contact the Planning Department if you have any questions.

We invite you to continue with Part Four!!!

**Project funded by a grant from the
State of Connecticut
Department of Economic and Community
Development**


Copyright LADA, P.C. Land Planners November 2020
All rights reserved
Aerial Photos used in base maps from CTECO website
All photos - Terri Hahn
A LADA, P.C. Land Planners Production via Zoom platform recording

Terri Hahn
LADA, P.C. Land Planners
104 West Street
Simsbury, CT 06070
Email: ladapc@snet.net

Warehouse Point -

Planning for Stormwater Management
and Village Center Redevelopment Grant
(Warehouse Point Planning Study)

Public Participation December 2020 Part 4 of 4



LADA, P.C., Land Planners

WELCOME!



- This is the fourth and final video presentations for the project known as
“The Planning for Stormwater and
Village Center Redevelopment Grant for
Warehouse Point”
OR
The Warehouse Point Planning Study
- The project is funded by a Responsible Growth and Transit Oriented Development Grant from the State of the Connecticut Department of Economic and Community Development (DECD)

This is a four part video series – each part runs approximately 20 minutes

- Part One- Why do this study and what is it about?
- Part Two- Understanding the Blue Ditch
- Part Three- Zoning and how it affects stormwater runoff
- **Part Four – What the Future of Warehouse Point could look like**

Comments on each segment can be posted on the Town's website at <https://www.eastwindsor-ct.gov/planning-zoning-commission/webforms/warehouse-point-planning-study>

The comments will be collected and addressed at the Planning and Zoning Commission meeting of December 29, 2020.

There is a survey that is coordinated with this video series

- The survey is posted on the Town website at <https://www.eastwindsor-ct.gov/planning-zoning-commission/webforms/warehouse-point-planning-study>
- Surveys were also mailed to property owners within the study area
- Please only fill out the survey once per person but anyone in your household can do so including your children!

The final part of this public participation event is the Planning and Zoning Commission meeting on December 29, 2020. A Final Report with recommendations will be completed in January 2021

Welcome!

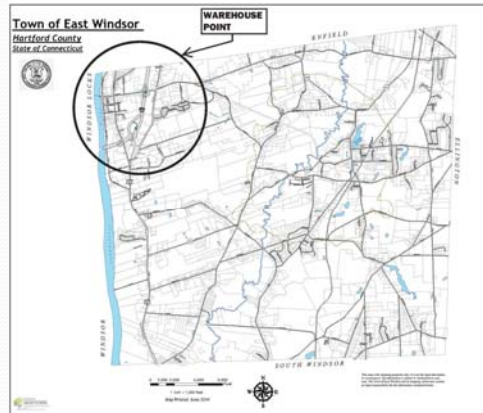


- Hi! My Name is Terri Hahn, Professional Landscape Architect
 - Licensed in CT and NY
 - Over 30 years of experience
 - Working in East Windsor for the last 5 + years

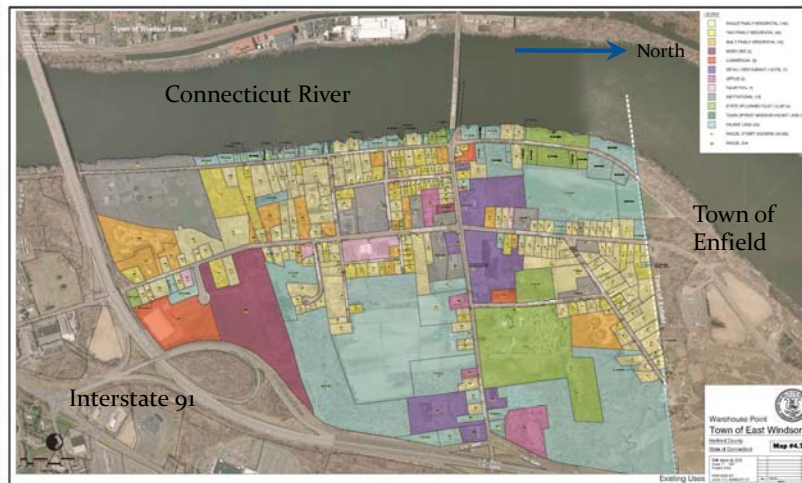
I am a Principal in the firm LADA, P.C. Land Planners from Simsbury, CT. We are the lead consultant for the project. We are Landscape Architects and Planners. The project engineer is Nathan L. Jacobson and Associates.

Introduction

- Where is the project?



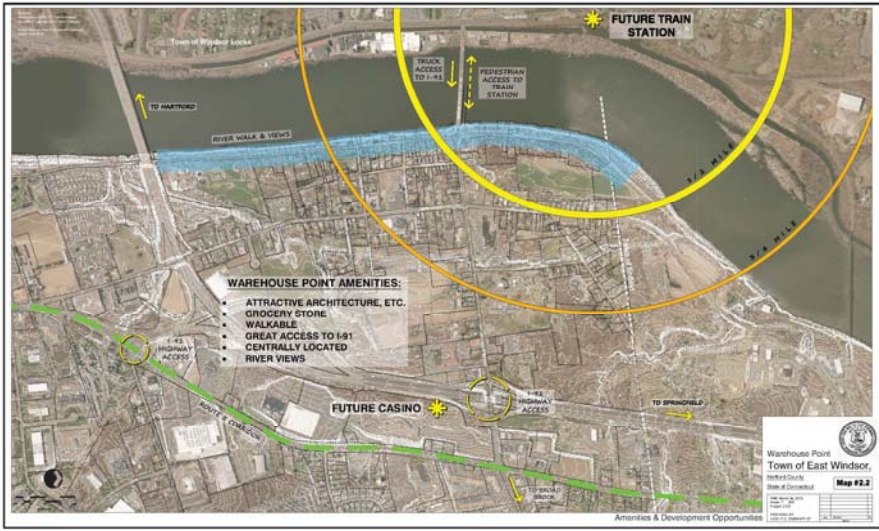
What parts of Warehouse Point are included in the study?



WHY IS THIS PROJECT IMPORTANT?



Why is this project important? Why did East Windsor get this grant?



Warehouse Point Planning Study- Video Presentations so far

- Part 1 – reviewed study limits
- Part 2- looked at stormwater runoff in Warehouse Point
- Part 3 – Looked at new uses for Warehouse Point
- Reviewed a number of Zoning Options relating to impervious surface coverage to see how they impact the Blue Ditch.
- Presented the Planning and Zoning Commission goals for the project

Part 4

- What should Warehouse Point become?
- Bike Paths and new sidewalks
- Gateway sign locations
- Possible development options for Bridge Street
- South Water Street redevelopment

What should Warehouse Point become?

Town of East Windsor Planning

- Town of East Windsor Plan of Conservation and Development completed in 2016 identified the following:
 - In the resident survey of 2014 – 70% of respondents suggested the villages should be designated with new zoning to promote business and tourism
 - Page 20 and Page 29- Warehouse Point is identified as a separate Primary Strategy Area to define future Village Area development
 - Pages 40-41 and 54-57 identify over 30 recommendations to encourage business, retail and restaurant expansion in Warehouse Point
 - New Residential uses to provide “boots on the street” to support the new businesses are also recommended on the upper levels of existing historic structures and new construction
 - Page 20 identifies a strong desire to keep the historic character of Warehouse Point while encouraging new development

What do we want Warehouse Point to become?

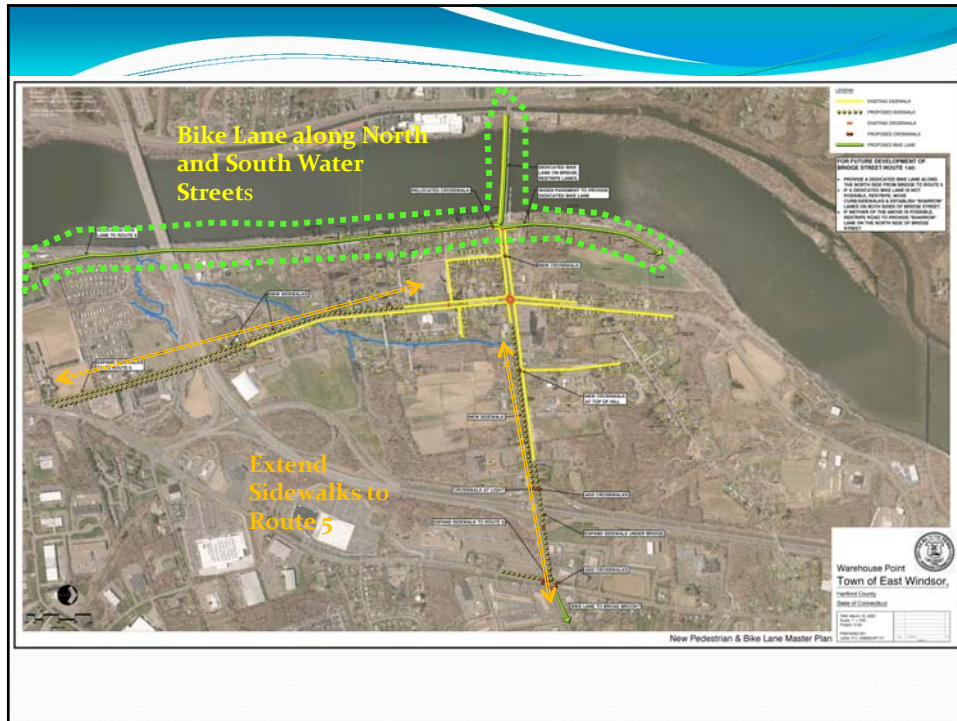
- Vibrant
- Exciting
- Some Place you want to go
- Some Place you want to live
- Walkable
- Connected
- A Place with History and a Future!



Future Sidewalks and Bike Lanes

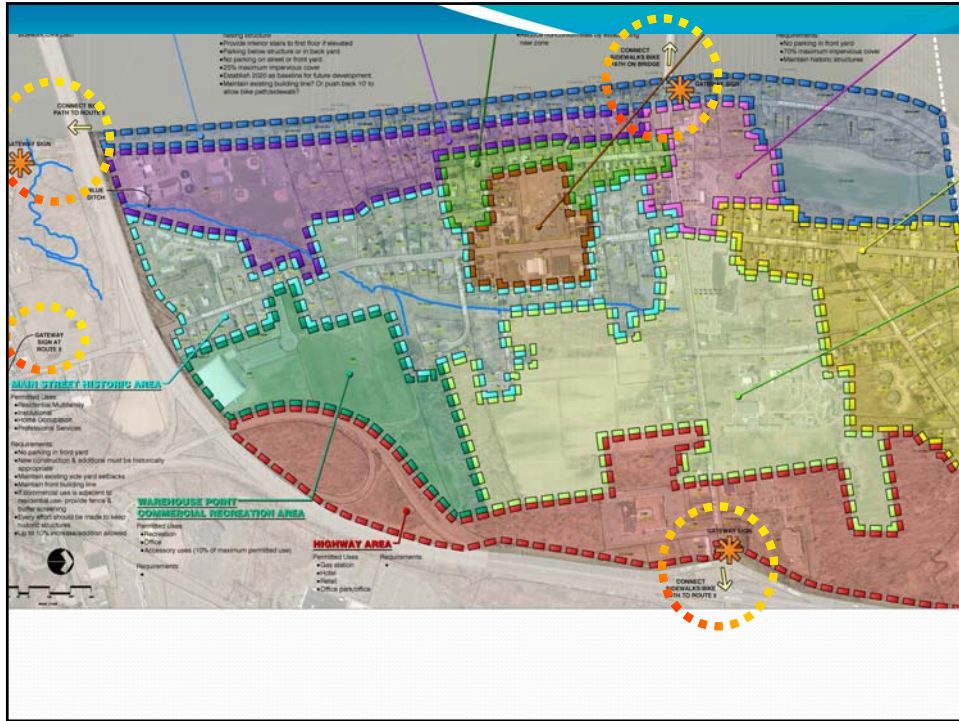
- Existing Sidewalks don't go far enough
- Develop future bike lanes with DOT as Bridge Street and Main Street are improved over time.
- Establish Bike/walking trails along South Water Street
- Establish walking trails to connect Main Street uphill to I-91 via private property





Gateway Signs and Connections beyond Project Area





Possible Development Options for Bridge Street

Bridge Street

- Add mixed use buildings on the north side of Bridge Street from Water Street to Main Street
- Maintain Historic Structures
- Maintain and Support important business such as Maine Fish and Geisslers Plaza
- Shared Parking
- No parking on Street or in front yard
- Set buildings along street to maintain historic “build-to” line
- Expand Sidewalks and add plazas, streetlights and planting

What might this look like?



What might this look like?



What might this look like?

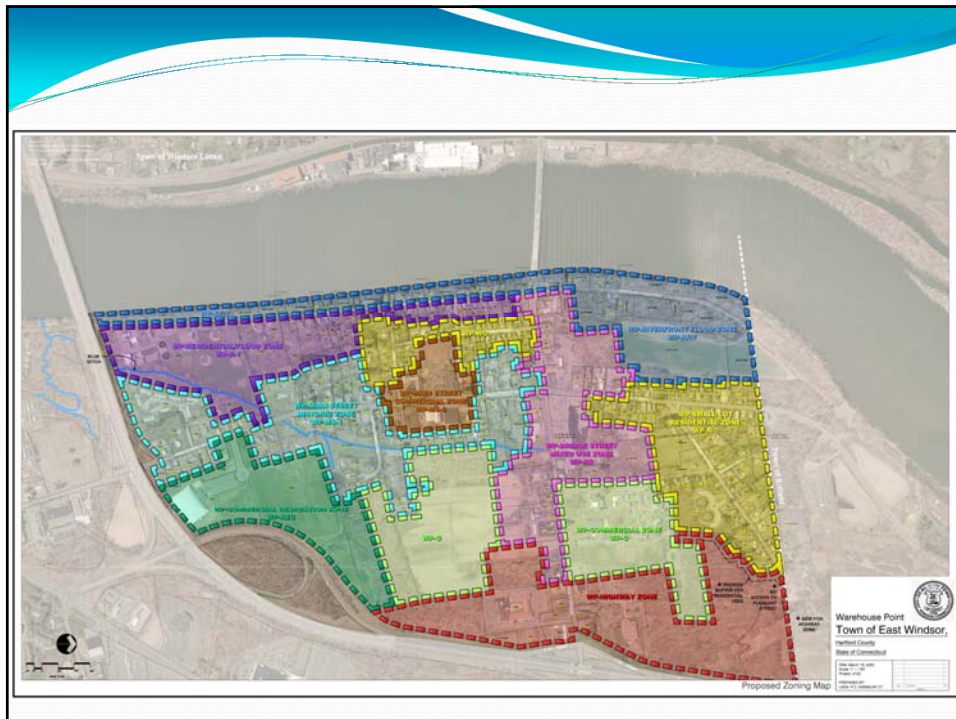


How does the town get what they want for future development/growth?

Creation of a new Zoning Regulation which defines the allowed uses and development requirements (Bulk Requirements)

Provides direction and guidance for new investment

Does not mandate change



So what happened to the Stormwater portion of the project?

New Zoning Regulation

- Identify allowed impervious surface in each area or zone of Warehouse Point
- Allow for use of pervious pavements and other “green infrastructure”
- Limit growth areas along the Connecticut River
- Connect Flood Regulations to Zoning Regulations
- Provides for protection of downstream properties while allowing new construction

Green Infrastructure

- Pervious Pavement
- Rain Gardens/Bioretention
- Streetscape infrastructure

Pervious Pavement

Permeable Block Pavers



Hole in the Wall Parking Lot – East Lyme, CT

Pervious Asphalt



UConn Storrs Campus – Mansfield, CT

Pervious Concrete



UConn Storrs Campus – Mansfield, CT

All images from
CT DEEP Website

Rain Gardens/Bioretention

Bioretention Areas



UConn Storrs Campus – Mansfield, CT

Image from CT DEEP Website



Images from epa.gov

Streetscape Infrastructure

Anatomy of a Green Street

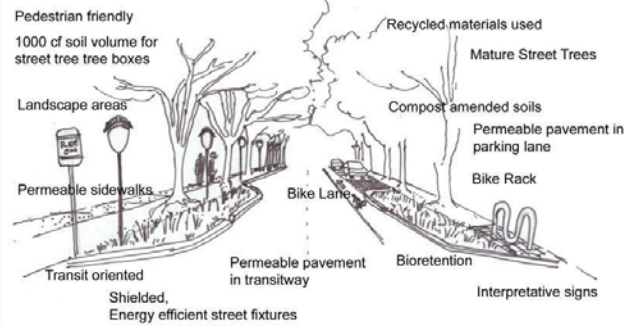


Image from epa.gov

Possible Development Options for South Water Street – east side only



South Water Street- ROAD IS AT ELEVATION 30+/-
Base Flood Elevation is 35'

One possible Vision

Widen road slightly

Set buildings back about 10-15' to create front yard

Add sidewalk to east side/Add bike lane along west side

Buildings would have upper level porches or balconies to take advantage of views to River

Maintain wooded buffer at rear yard

Raise buildings up above flood elevation

Park under building or in rear yard

This vision would require cooperation from existing property owners



Questions to consider:

1. What do you think?
2. Will the proposed changes to the Zoning Regulations create a place you would visit? Live? Go to a store? Get a meal?
3. Do you like what you see?

This is the end of this four part video series on the Warehouse Point Planning Study. You may watch the previous versions again by visiting the Town Website. Comments may be made on the page where you found this video.

Thank you for your time!

- Comments can be made on the East Windsor Town Website-

<https://www.eastwindsor-ct.gov/planning-zoning-commission/webforms/warehouse-point-planning-study>

Comments will be collected from the Town website and addressed at the Planning and Zoning Commission meeting on December 29, 2020 at 6:30pm. Please check the website for Zoom log-in instructions. If you do not have Zoom, the Town posts all meetings after those meetings. You may contact the Planning Department if you have any questions.

**Project funded by a grant from the
State of Connecticut
Department of Economic and Community
Development**

Copyright LADA, P.C. Land Planners November 2020
All rights reserved
Aerial Photos used in base maps from CTECO website
All photos - Terri Hahn
A LADA, P.C. Land Planners Production via Zoom platform recording


Terri Hahn
LADA, P.C. Land Planners
104 West Street
Simsbury, CT 06070
Email: ladapc@snet.net

APPENDIX F

Warehouse Point -

Planning for Stormwater Management
and Village Center Redevelopment Grant
(Warehouse Point Planning Study)

Public Participation- Final Q & A December 2020




LADA, P.C, Land Planners

WELCOME!



- This is the final public presentation for
“The Planning for Stormwater and
Village Center Redevelopment Grant for
Warehouse Point”
OR
The Warehouse Point Planning Study
- The project is funded by a Responsible Growth and Transit Oriented
Development Grant from the State of the Connecticut Department of
Economic and Community Development (DECD)

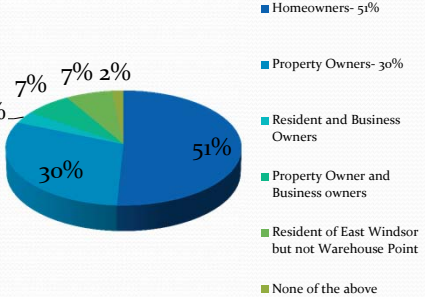
A survey was prepared and mailed to every property owner within the project area. In addition, an on-line survey was conducted via Survey Monkey via a link on the Town's website.



Survey Results

- 250 surveys mailed
 - 42 paper copies received
 - 2 phone calls
 - 6 on-line responses
 - 50 total responses
 - Men 50%/Women 50%
 - 15 surveys returned
 - Not all questions answered

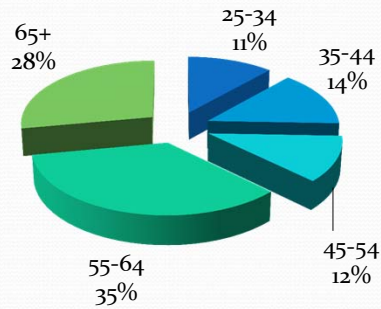
Are you a resident/owner in Warehouse Point?



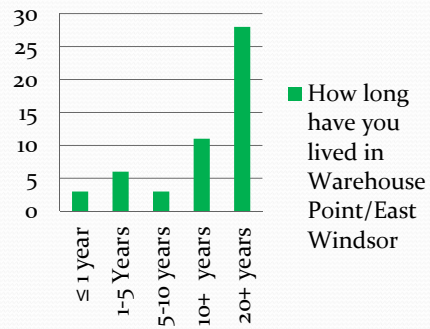
Category	Percentage
Homeowners	51%
Property Owners	30%
Resident and Business Owners	7%
Property Owner and Business owners	7%
Resident of East Windsor but not Warehouse Point	2%
None of the above	3%

Survey Results

Your Age

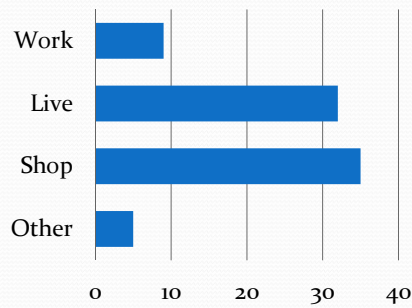


How long have you lived in Warehouse Point/East Windsor?

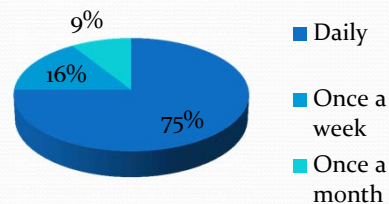


Survey Results

Do you work, shop or live in Warehouse Point? (could answer more than once)



How often do you visit Warehouse Point?



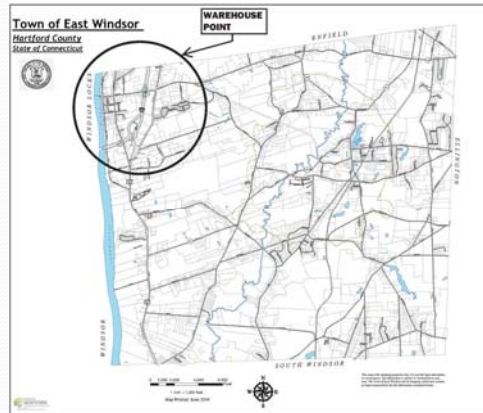
There is a four part video series –
each part runs approximately 20-35
minutes located on the Town’s website

- Part One- Why do this study and what is it about?
- Part Two- Understanding the Blue Ditch
- Part Three- Zoning and how it affects stormwater runoff
- Part Four – What the Future of Warehouse Point could look like

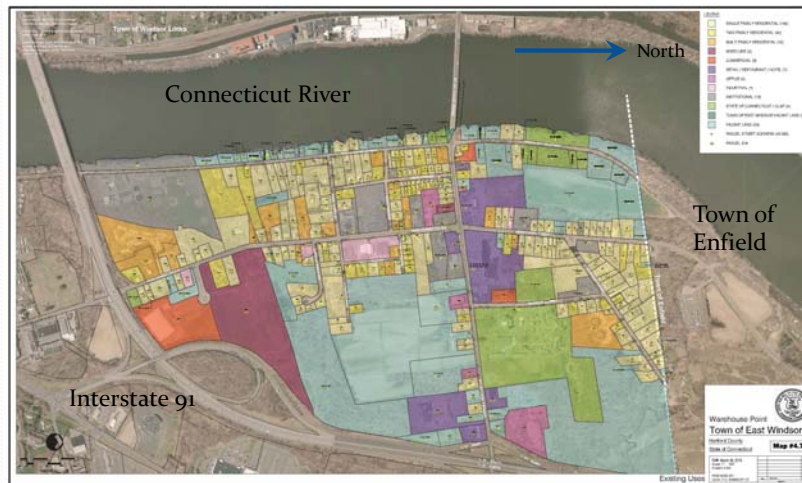
**This is the final part of this
public participation event.
A Final Report with
recommendations will be
completed in January
2021**

Introduction

- Where is the project?



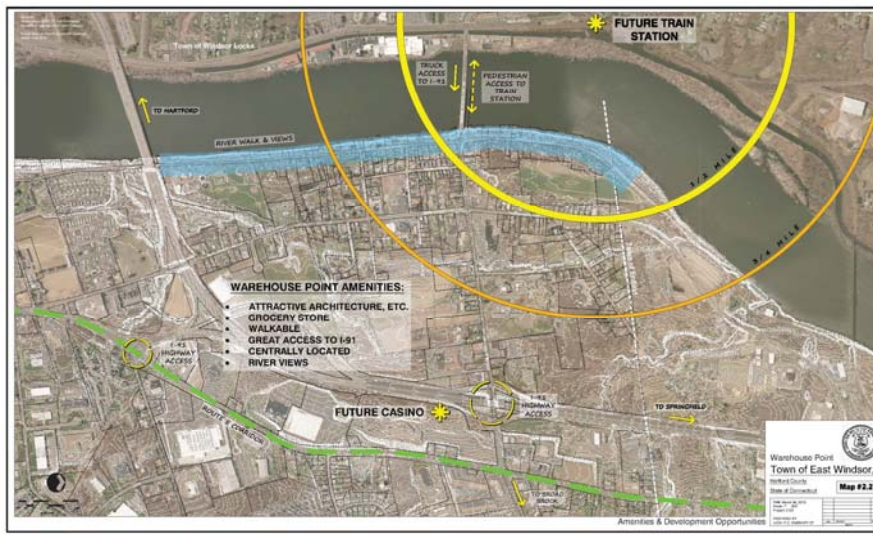
What parts of Warehouse Point are included in the study?



WHY IS THIS PROJECT IMPORTANT?

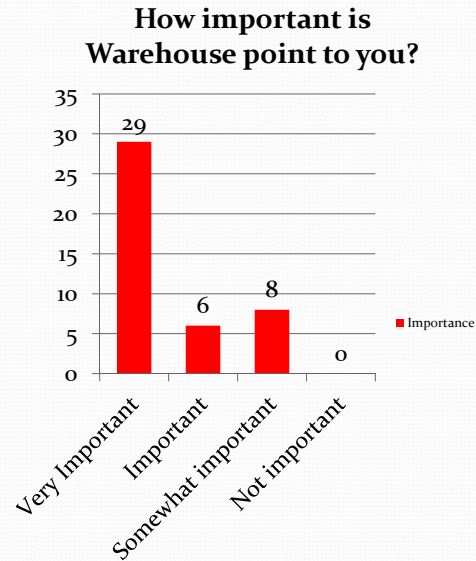


Why is this project important? Why did East Windsor get this grant?



Survey Results

- Every respondent indicated that Warehouse Point was important to them



Survey Results –

Use of the Train Station may not be an idea residents embrace yet!!!

- **Is access to the train station important to you?**
 - 50% neutral or N/A
 - Of remaining – 25% agree and 25% disagree
- **I would take the train to work**
 - Only 10% agree, everyone else says No!
- **I would give up my car and take the train**
 - A resounding 80% says no!!

Summary, Public Comments and Next Steps

- What should Warehouse Point become?
- Ideas, Images and public feedback
- What comments and questions do residents have?
- Preliminary recommendations
- Next steps

Question from Survey Participants

- Who is paying for this project?-
 - The project is funded by a Responsible Growth and Transit Oriented Development Grant from the State of the Connecticut Department of Economic and Community Development (DECD)

Question from Survey Participants

- Why are you doing this project?

Town of East Windsor Planning

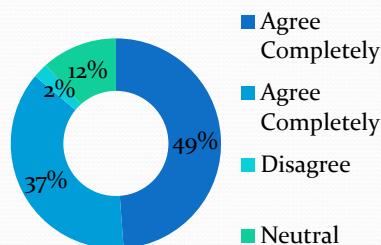
- Town of East Windsor Plan of Conservation and Development completed in 2016 identified the following:
 - In the resident survey of 2014 – 70% of respondents suggested the villages should be designated with new zoning to promote business and tourism
 - Page 20 and Page 29- Warehouse Point is identified as a separate Primary Strategy Area to define future Village Area development
 - Pages 40-41 and 54-57 identify over 30 recommendations to encourage business, retail and restaurant expansion in Warehouse Point
 - New Residential uses to provide “boots on the street” to support the new businesses are also recommended on the upper levels of existing historic structures and new construction
 - Page 20 identifies a strong desire to keep the historic character of Warehouse Point while encouraging new development

Survey Results-What do you consider to be the best aspect of Warehouse Point? (In order of interest)

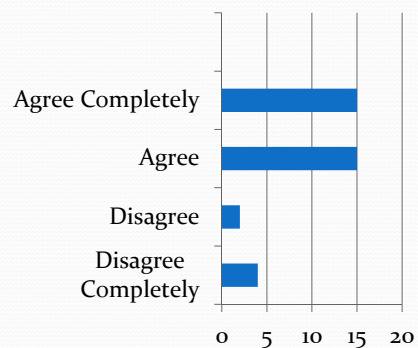
- Small town feeling
- Connecticut River
- Easy to walk to stores
- Access to I-91
- Town Green
- Local stores, local services
- Historic buildings
- Library
- Cleanliness

Survey Results

The Historic buildings in Warehouse Point are important to me



I think the historic buildings should be reused/restored/renovated as something new



Note: Respondents who disagreed felt question meant removing and replacing buildings

Question: The grocery store is important to Warehouse Point-

99% of respondents agreed (2 were neutral)

Study Response-

- Site Plan studies to expand Geisslers plaza uses
- Look at current Zoning Regulations to support growth of existing uses such as Maine Fish
- Provide incentives in Proposed Zoning to support existing uses but provide for new growth for those who want to change
- Maintain existing building line on street
- Support preservation of historic structured by modifying setbacks.

Study Response-

- Acknowledgement of preference for existing historic building types.



Survey Results-What do you consider to be the least appealing aspect of Warehouse Point?

(In order of interest)

- Blighted buildings
- Homeliness/Drugs
- Traffic/trucks
- Not always feel safe walking
- Not enough to do/shopping
- Empty Space
- Fire Tax
- School District
- Town Politics
- New project on Wagner
- Fire Alarm noise

Survey Results- What would you encourage you to visit Warehouse Point more often?- (In order of interest)

- **Restaurants (50% agree, 10% disagree)**
- **More small businesses (40% agree, 15% disagree)**
- **Coffee shop**
- **Develop Riverfront as Walk/Park along river (70% agree, 10% disagree)**
- **Improve Blighted properties**
- **Better sidewalks**
- **Bike Lane**

Are you taking away my House?

No, this is a planning study.

The ideas presented here, if implemented, would not require that you give up your house or be forced to leave.

This study shows a possible future but does not mandate or require you to do anything different.

It does consider possibilities for the future to gauge whether the ideas are appropriate or residents are interested.

Question from Survey Participants

- Is this project just for Bridge Street?

**What should Warehouse
Point become?**

Survey Results- The Future of Warehouse Point

- More shops but not shopping plazas
- Better streetscape
- Don't have a negative impact on existing stores
- Reroute truck traffic
- Develop loan/grant program to help people update their historic properties
- Restore historic properties not replace
- Approximately 10% of respondents felt that Warehouse Point is fine the way it is and there should be no change

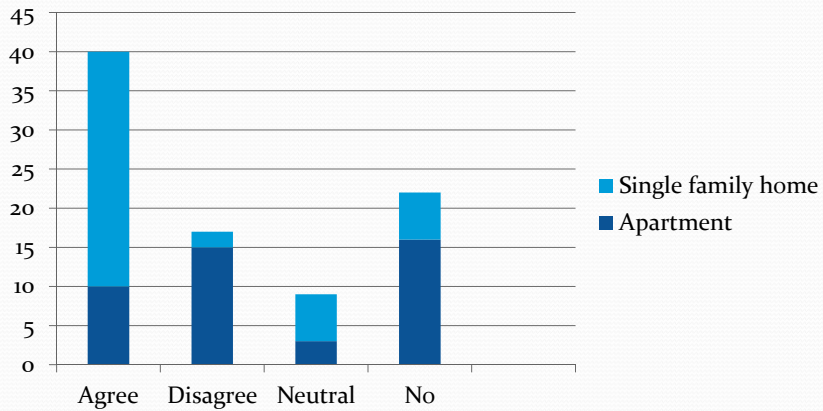
What do residents want Warehouse Point to become?

- Vibrant
- Exciting
- Some Place you want to go
- Some Place you want to live
- Walkable
- Connected
- A Place with History and a Future
- Survey results – 2 questions-
 - 58% would like to see new development in Warehouse Point/ 23% do not
 - 65% disagree that nothing should change/ 14% agree



Survey Results- The Future of Warehouse Point - Residential

- Respondents were clear that apartments hold limited appeal in Warehouse Point (Responses were somewhat age dependent)

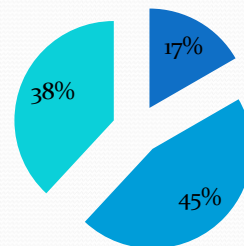


Survey Results- The Future of Warehouse Point - Business

- Respondents were not confident that any one would want to start a new business in Warehouse Point-

Question- I would like to open a business in Warehouse Point

■ Agree ■ Disagree ■ Neutral



Survey Results- Streetscape

Should be

- Outdoor Dining on Bridge Street and Main Street
- Should be walkable from street and between businesses
- Maintain the historic buildings
- Bike lane
- Dog and Family Friendly

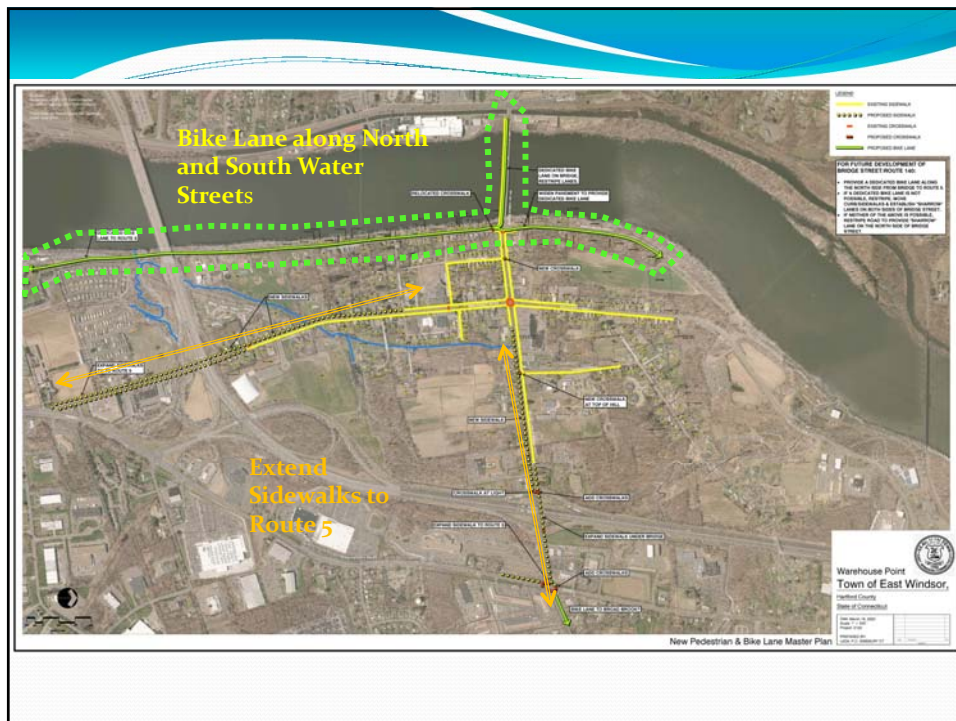
Need to address

- Repair the sidewalks now
- Traffic
- No place to put snow

Study Response-

Future Sidewalks and Bike Lanes

- Existing Sidewalks don't go far enough
- Develop future bike lanes with DOT as Bridge Street and Main Street are improved over time.
- Establish Bike/walking trails along North and South Water Street
- Establish walking trails to connect Main Street uphill to I-91 via private property



Possible Development Options for Bridge Street

Bridge Street

- Add mixed use buildings on the north side of Bridge Street from Water Street to Main Street
- Maintain Historic Structures
- Maintain and Support important business such as Maine Fish and Geisslers Plaza
- Shared Parking
- No parking on Street or in front yard
- Set buildings along street to maintain historic “build-to” line
- Expand Sidewalks and add plazas, streetlights and planting

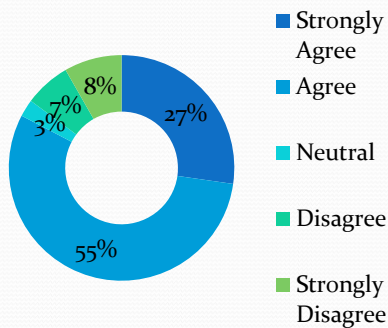
What might this look like?



Survey Results





I would visit/shop/walk here



Other Comments-

- Outdoor Dining should be allowed
- Maintain Historic buildings
- Keep Maine Fish
- Needs a fresh look without too much change
- Traffic is bad now- no new development
- Its not right to take someone's home

What might this look like?

Survey Results

I would visit/shop/walk here



- Strongly Agree
- Agree
- Disagree
- Strongly Disagree
- Neutral

Other Comments-

- Outdoor Dining should be allowed
- A safe way to walk/bike across the bridge is a good idea
- Maintain Historic buildings
- Great Idea!
- Need to reduce truck traffic
- The rotary is gone, right?!
- Walking across the bridge is good right now- don't need to change
- Not sure we like architecture

Question from Survey Participants

- Why are you proposing a Blue Back Square style development in Warehouse Point?
- We'd rather be Collinsville than West Hartford

What might this look like?



How does the town get what they want for future development/growth?

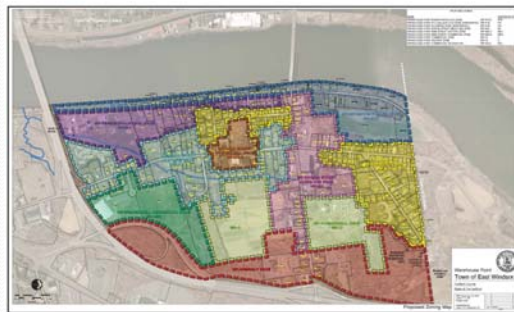
Creation of a new Zoning Regulation which defines the allowed uses and development requirements (Bulk Requirements)

Provides direction and guidance for new investment

Does not mandate change

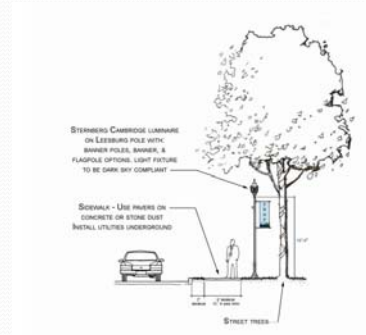
Study Response-

- Develop New Zoning Map and modify Zoning Regulations to add Warehouse Point zones to allow development of this kind



Study Response-

- Develop Streetscape cross-sections to describe preferred sidewalk layout at specific locations



Question from Survey Participants

- This is great but who is going to pay for all of this??

So what happened to the Stormwater portion of the project?

Survey Results

- Question –
 - Stormwater runoff is a problem in Warehouse Point
 - Agree 45%
 - Disagree – 18%
 - The flooding problems are so bad in Warehouse Point – we should look elsewhere
 - Agree- 12%
 - Disagree- 59%
- I live in Warehouse Point and constantly have a flooding problem
 - Agree - 22%
 - Disagree - 44%
- I have seen or experienced Connecticut River flooding
 - Agree - 80%
 - Disagree – 5%
- I have seen or experienced Blue Ditch flooding
 - Agree – 37%
 - Disagree – 22%

Survey Results

- Question –
 - New Development should not increase runoff
 - Agree- 65%
 - Disagree- 10%
 - New Construction should be done in such a way that it does not impact downstream neighbors
 - Agree – 80%
 - Disagree – 5%
- I am concerned that new building will result in creased flooding
 - Agree - 45%
 - Disagree – 23%
- Having new business and places to live is worth increased runoff
 - Agree - 17%
 - Disagree – 59%
- I want both new development and no increased flooding
 - Agree – 68%
 - Disagree – 25%

Survey Results

- Question –
 - I am willing to use tax dollars to reduce flooding in Warehouse Point and elsewhere in Town
 - Agree- 55%
 - Disagree- 26%

Question from Survey Participants

- Why can't you just maintain the Blue Ditch?
- What can you do about the Connecticut River?- I am losing property every year.
- Need to clean out the culvert/replace culverts at Spring and Holcomb
- What are you doing about adding storm drains on the street?
- What can you do about maintaining vegetation along River?

Study Response- New Zoning Regulation

- Identify allowed impervious surface in each area or zone of Warehouse Point
- Allow for use of pervious pavements and other "green infrastructure"
- Limit growth areas along the Connecticut River
- Connect Flood Regulations to Zoning Regulations
- Provides for protection of downstream properties while allowing new construction

Green Infrastructure

- Pervious Pavement
- Rain Gardens/Bioretention
- Streetscape infrastructure

Pervious Pavement

Permeable Block Pavers



Hole in the Wall Parking Lot – East Lyme, CT

Pervious Asphalt



UConn Storrs Campus – Mansfield, CT

Pervious Concrete



UConn Storrs Campus – Mansfield, CT

All images from
CT DEEP Website

Streetscape Infrastructure

Anatomy of a Green Street

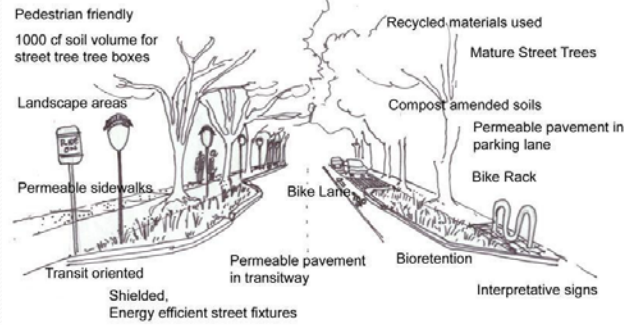


Image from
epa.gov

Possible Development Options for South Water Street – east side only



**South Water Street- ROAD IS AT ELEVATION 30+/-
Base Flood Elevation is 35'**

One possible Vision

Widen road slightly

Set buildings back about 10-15' to create front yard

Add sidewalk to east side/Add bike lane along west side

Buildings would have upper level porches or balconies to take advantage of views to River

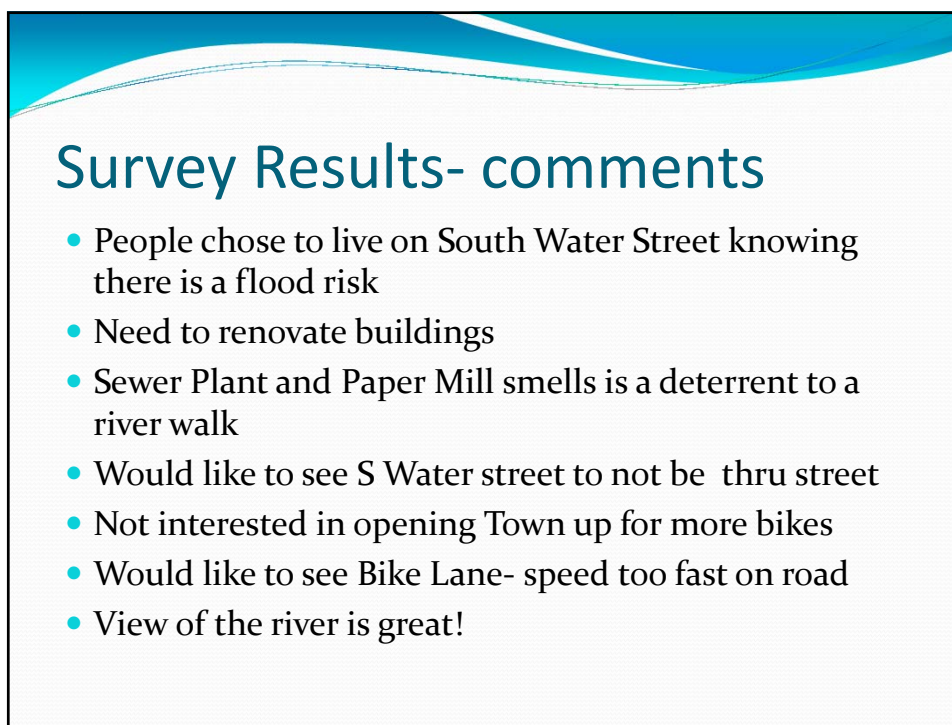
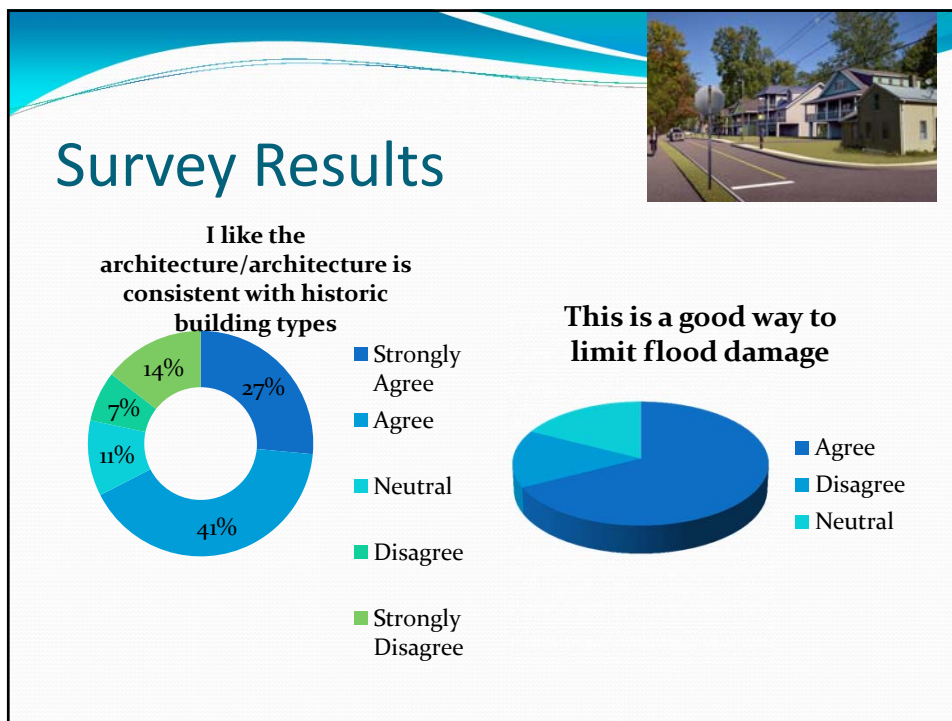
Maintain wooded buffer at rear yard

Raise buildings up above flood elevation

Park under building or in rear yard

This vision would require cooperation from existing property owners





FINAL THOUGHTS FROM SURVEYS

Survey Results- Final Thoughts

- New Architecture should match existing styles
- Fix what exists
- We like the proposed Architecture but keep it small- no big boxes
- Outdoor dining and more restaurants and shops
- Restore Older homes
- Maintain village look
- Quality development with nice streetscape
- Electric vehicle Friendly
- Dog and family friendly
- Stop asking questions- Just do something!!!!

Survey Results- Final Thoughts

- Clean up Blighted properties
- Not sure that past experience bodes well for future
- We don't need any more commercial growth – put on Route 5
- Not apartments
- Make the area more upscale
- Need a Town Dump/Recycling pick up area in Warehouse Point
- Market should drive development – stop trying to plan everything
- We don't need any of this – it works fine the way it is
- Renovate and Use the Annex/Old School
- Maintain strong connections to Broad Brook and Windsor Locks

Were any issues identified that would affect future or existing quality of life?

Yes

- Homelessness
- Buildings in disrepair
- No activity along the street
- Truck Traffic

Not convinced anyone will want to start/open business in Town

- Reduce Speed limit on South Water Street

Draft Recommendations

- Finalize and adopt the proposed Warehouse Point Zoning Regulations. Update the definitions as noted (Mixed Use, Build-to Line, etc). Modify the Zoning Map and use tables to reflect the Warehouse Point Zones.
- Establish Design Guidelines for Warehouse Point with typical light poles and other street furniture to set the style for each area.
- The preferred Building Type for Warehouse Point is one where the building looks like the single-family and multi-family buildings of the period when Warehouse Point was at its heyday- from 1840-1910. These buildings and their significant architectural features are described in detail in this report.
- The existing 1840-1910 architecture is considered to be visually attractive and should be used as style precedents for future architectural guidelines. Architectural guidelines should be developed for both commercial and residential new construction which are consistent with this preferred style. These guidelines should also include elements which address additions and rehabilitation of existing buildings.
- The use of period-appropriate ornament (materials, shutters, gingerbread, colors, etc.) is preferred to a plain façade.

Draft Recommendations

- Parking and Garages, when provided, are to be located behind the main building. Parking directly on Bridge Street and Main Street should be limited to special events only.
- Roofline changes, gable ends facing the street, dormers and other period-appropriate methods to reduce the view of the roof from the street is preferred.
- Mixed use (commercial and residential) is recommended along Bridge Street, in Warehouse Point. Mixed Use in this case is defined as commercial (retail preferred) on the street level and office or residential on second story.

Draft Recommendations

- Increased residential density should be focused in Warehouse Point but within the existing fabric (layout and density) of past development patterns.
- Work with the Capital Region Council of Governments to include Warehouse Point as a priority TOD development area associated with the TOD designation of the Windsor Locks inter-city rail stop identified in the Jonathon Rose market analysis for the Knowledge Corridor. Encourage bike and pedestrian linkages to the rail stop across the Route 140 bridge by working with CRCOG, ConnDOT and Windsor Locks to expand the existing minimal pedestrian access and environment on the bridge and into Warehouse Point. Designate a representative to serve on the committees and delegations associated with the Windsor Locks station improvements.
- Work with DOT to extend storm drainage along Bridge Street directly to the Connecticut River. Coordinate with DOT during next regular maintenance of road to expand sidewalks and establish bike lanes along Bridge Street and Main Street to Route 5.

Draft Recommendations

- The CRCOG Regional Plan concludes that Millennials, now entering the workforce, “will be looking for compact, walkable, mixed use communities with a selection of restaurants, shops, services and cultural amenities. They are a generation that embraces transit over the automobile and will choose housing based upon what transportation options exist.” The CRCOG Regional Plan also concludes that 89% of aging “Baby Boomers” prefer to live in place (same home or community), and in many cases downsize to live in vibrant, walkable mixed use communities. There will be a significant reduction in demand for large suburban homes. In light of those predictions, East Windsor has established mixed use zoning (village style development) for Warehouse Point, with architectural guidelines to govern infill construction, new construction and rehabilitation
- Encourage restaurant uses within Warehouse Point areas to support the residential neighborhoods. Consider the development of village parking plans (showing shared and common parking) and on-street parking where an individual site may not be able to meet the Town’s parking requirements. The new draft Zoning Regulations include reduced parking standards and an emphasis on Shared Parking. The Town may need to establish a parking district to address insurance and liability concerns as the public is invited to use property not directly related to their destination.

Draft Recommendations

- Coordinate impervious and Zoning to define the level of impervious surface which can be handled in these zones and what the requirements for water quality, and stormwater control would be for individual applications and the area as a whole to supplement and offset the requirements of individual sites where possible and feasible. Flooding and stormwater issues related to the Blue Ditch are addressed in a an overall Stormwater Master Plan which includes Town or DOT installed and controlled facilities. This would include the need to acquire easements for property where the Blue Ditch travels through to provide regular maintenance and future improvements to reduce flooding.
- Streetscape improvements to improve pedestrian and bicycle connections between the main roads and neighborhoods should be the subject of future engineered plans and grant application, if available. The improvements should include sidewalks sized for multiple people (5' minimum), bike lanes, benches, light fixtures and changeable elements such as hanging plants and banners. Where possible, seek inclusion in the regional ConnDOT Transportation Improvement Plan (TIP), and ultimately the Statewide TIP to be able to access Federal funds for improvements.

Draft Recommendations

- Consider incentives for second and third floor residential units within existing structures in return for maintaining the historic facades and materials of buildings built prior to 1950.
- As noted in previous portions of the Plan of Conservation and Development, there is existing farm land located in Warehouse Point that could provide a significant resource in the future. The development of a Farmers Market and/or clear and simple farmstand regulations may allow additional land to become viable as farmland including the currently very popular community supported agriculture (CSAs) which allow residents to share in the benefits and risks of farming.
- As noted in previous portions of the Plan of Conservation and Development - create a Town-wide Bicycle and Pedestrian Plan which allows for the connection of Broad Brook and Warehouse Point with Route 140 and possible the trolley line. The bike path should also extend down North and South Water Streets to provide a River's Edge Bike Path. Extensions of the bike path along Bridge Street should be continued along Route 140 to Broad Brook and extension along Main Street should provide for continued connections to Route 5 and the other portions of Town.

Draft Recommendations

- Where possible, seek funds to promote appropriate redevelopment or elimination of flood prone residential buildings as identified in the proposed Riverfront Zone. Seek State Open Space funds and other available funding to preserve the land along the Connecticut River through the purchase of these lands and removal of residential uses along the river side of North and South Water Streets where property owners are willing.
- Coordinate with the WPCA to allocate capacity of the existing waste water plant , pipes and pump stations for new growth in Warehouse Point. Develop a long term program for infrastructure expansion and maintenance to avoid excess connection fees for future growth.
- Develop gateway elements for Warehouse Point that creates a sense of arrival and defines the character of the village. The locations of future gateway signs are shown on the plans.

Draft Recommendations

- Modify regional and state PoCDs that will be required to correspond to and reflect those areas where future increased density is desired by the Town.
- Maintain the culvert at Bridge Street and the drainage way from Bridge Street to Spring and Holcomb streets by coordinating with contractor. Wetland and ACOE permitting may be required.
- Submit for grants to coordinate the creation of a façade improvement loan program to address updates to retail/commercial buildings and historic assets in Warehouse Point
- Work with Town Police and Social Services to address homelessness issues in Warehouse Point.
- Others????

Next Steps

- Prepare draft Final Report with recommendations- Due January 7, 2021
- Draft Zoning Map and Zoning Regulations
- Draft Stormwater regulations integrated into Zoning Regulations
- Planning and Zoning Commission to review and edit – January 12, 2021
- Board of Selectmen review- January meeting
- Submit Final Report- January 21, 2021
- Planning and Zoning Commission to accept Final Report – January 26,2021

**Any other
Public Comments?**

**Project funded by a grant from the
State of Connecticut
Department of Economic and Community
Development**

Copyright LADA, P.C. Land Planners December 2020
All rights reserved
Aerial Photos used in base maps from CTECO website
All photos - Terri Hahn

Terri Hahn
LADA, P.C. Land Planners
104 West Street
Simsbury, CT 06070
Email: ladapc@snet.net